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 L. Everingham & Co., grain, seeds.
 Sam Finney, commission.
 W. A. Fraser Co., grain commission.
 Gerstenberg & Co., grain, seeds.
 H. Hemmelgarn & Co., commission.
 Lowell Hoit & Co., grain, hay.
 Kerrick, Gray & Williams, grain comsn.
 W. F. Johnson & Co., grain, seeds.
 Lasier & Hooper, rcvs and shprs.
 L. H. Manson & Co., grain, comsn.
 W. H. Merritt & Co., grain, seeds.
 W. R. Mumford Co., commission.
 Geo. H. Phillips, grain commission.
 Pope & Eckhardt Co., commission.
 H. W. Rogers & Bro., grain and seeds.
 Rosenbaum Bros., receivers, shippers.
 J. Rosenbaum Grain Co., rcvs, shprs.
 Scribner, Crighton & Co., commission.
 Rumsey & Company, grain com's'n.
 James P. Smith & Co., grain.
 Van Ness & Wilson, grain receivers.

CHICAGO—Continued.

E. W. Wagner, receiver and shipper.
 Ware & Leland, grain, seeds.
 Weare Commission Co., commission.

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Union Grain & Hay Co., grain, hay.

COLUMBUS, O.

Columbus Grain and Eltr Co., grain, oats.
 Percy R. Hynson, buyer and shipper.
 Jas. P. McAjister & Co., grain and hay.
 McCord & Kelley, track buyers, shippers.
 Scott & Woodrow, grain and hay shippers.
 Seeds Grain Co., grain and hay.
 Tingley Bros., grain, hay, chop feed.

DAYTON, OHIO.

Schaeffer & Boroff, grain shippers.

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W. F. Morgan, grain broker.

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C. E. Burns, oats, rye, beans.

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W. H. Small & Co., grain, seeds.

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Andrews & Ranson, grain, hay, etc.

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Ernst-Davis Grain Co., commission.
 Robert I. Thresher, grain brkr & comsn.

KENTLAND, IND.

McCray, Morrison & Co., track buyers.

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John Wade & Sons, grain dealers.

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 I. H. Lowry & Co., grain com'nsn.
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C. A. Foster, grain, hay, feed.
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 Connor Bros. & Co., grain comsn.
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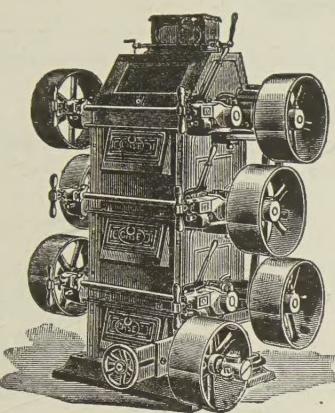
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THEY HAVE MORE CAPACITY, BUILT STRONGER, CLIP AND SCOUR WITH LESS WASTE, AND DO BETTER WORK THAN ANY OTHER. CAN MAKE PROMPT SHIPMENTS.

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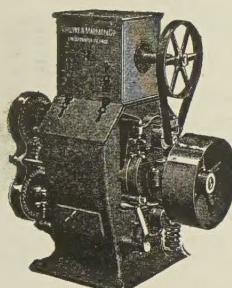
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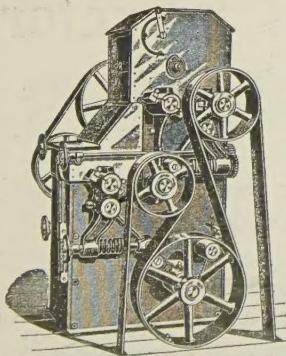
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We may safely claim the Northway Feed Mill to be the greatest money maker on the market, because of its immense capacity, thorough construction and care and accuracy of adjustments.

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Is unequalled as a strong, steady, reliable mill. It is made with four or six rollers, and may be driven from either side. Headquarters for Elevator and Flour Mill Supplies.

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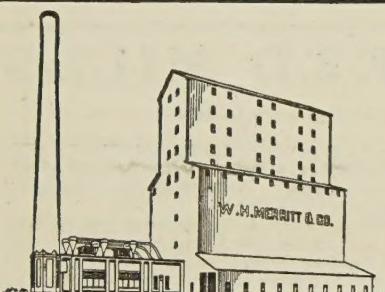
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GRAIN ELEVATORS
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Made to order. While we do not carry them in stock, we can usually make very "prompt delivery." If you want one and want a GOOD one, let us prepare your plans and furnish the equipment. Use our "Aspirator" Boot and save expense in handling grain. Correspondence solicited.

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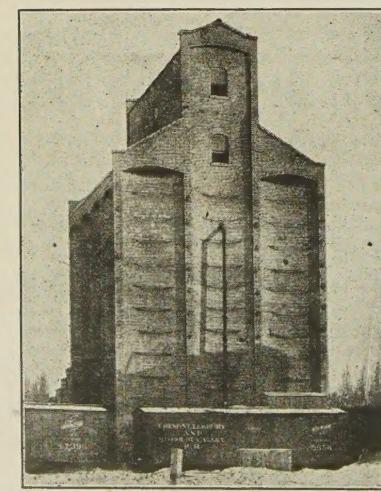
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Fire Hose,

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Affords you the easiest and quickest transit between first and cupola floors, known to mechanical science. It needs no power and, therefore, is always ready. Please inform yourself as to its merits by applying to

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Complete Elevator Outfits a Specialty

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Cleaning Machinery
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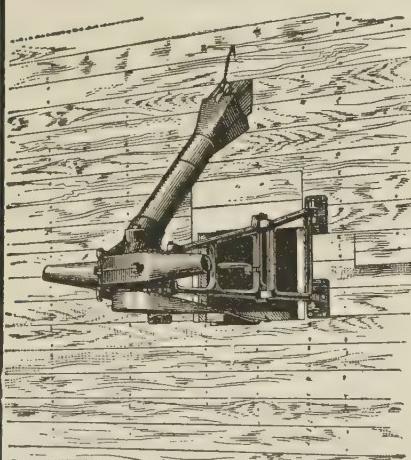
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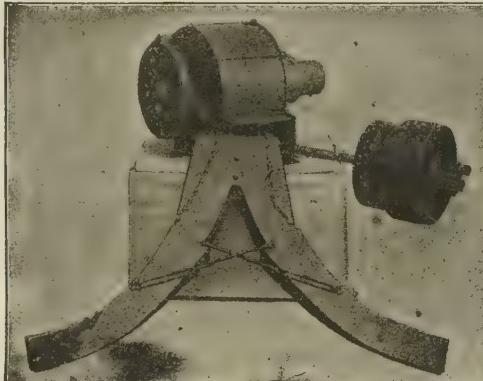
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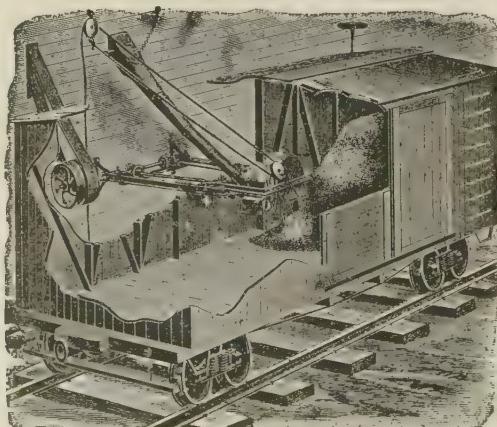
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Loads both ends of car
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Will fill each end of the car at the same time.

Has a capacity of 6,000 to 9,000 bushels per hour.

A perfect success.

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A GRAIN SPOUT

That will load cars without shoveling.

It is worth its weight in gold.

It will save you in labor all it costs in less than a month.

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Siebers Bros., Buckley, Ill.: "Discontinue our adv., as we have sold the engine. Have had many inquiries."

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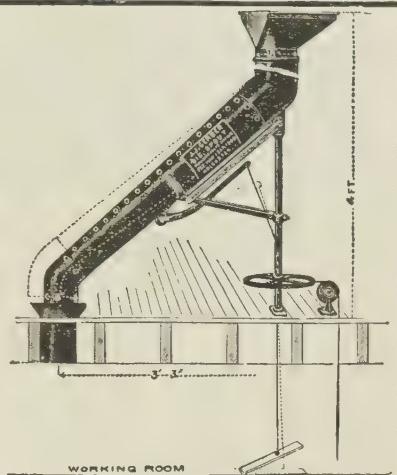
in your elevator. If not, try one and be convinced that it is the best Spout you can have in your elevator.

Patented May 15, 1900, Feb. 18, 1902.
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Are you running an Elevator?

Are you an up-to-date business man?

If so you are using the

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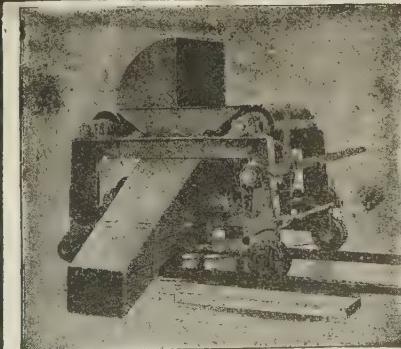
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All Kinds Rebuilt and Repaired Promptly.

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STEEL SPOUTING



It is to carry grain to any point. Send for prices and further particulars to the manufacturers,

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For taking grain from the head of an elevator this spouting is far superior to the ordinary wooden spouts. A glance at the cut will show how handy

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ELEVATOR BUCKETS

The Fuller Patent Steel Bucket is stronger, will carry more and make better delivery than any other.



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Perfectly protects
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From corrosion for years. Write us for
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Clark's Grain Tables for Wagon Loads are used more extensively by country grain buyers than all other tables combined for finding the number of bushels in any wagon load. It contains nine tables for this purpose and four dockage tables, all printed from heavy faced type, in two colors. Each table gives reductions from 100 to 4,000 lbs., is complete on one page, and is so arranged as to provide an effectual safeguard against errors by inexperienced persons.

The tables show the following reductions: (1) OATS at 32 lbs.; (2) OATS at 33 lbs.; [A table reducing oats to bushels of 33 lbs. will be substituted for the 32 lb. table if desired.] (3) BARLEY, HUNGARIAN GRASS SEED and CORN MEAL at 48 lbs.; (4) SHELLLED CORN, RYE and FLAXSEED at 66 lbs.; (5) WHEAT, CLOVER SEED, BEANS, PEAS, and POTATOES at 60 lbs.; (6) EAR CORN at 70 lbs.; (7) EAR CORN at 75 lbs.; (8) EAR CORN at 80 lbs.; (9) TIMOTHY SEED at 45 lbs.

The four dockage tables show the dockage on 60 pound Wheat at 1, 2, 3 and 5 pounds dockage.

The tables are bound in strong Manila covers.

Price, 50 cents. Address

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The grain trade's accepted medium for "Wanted" and "For Sale" ads. is the GRAIN DEALERS JOURNAL of Chicago, because such ads. placed in it bring quick returns.

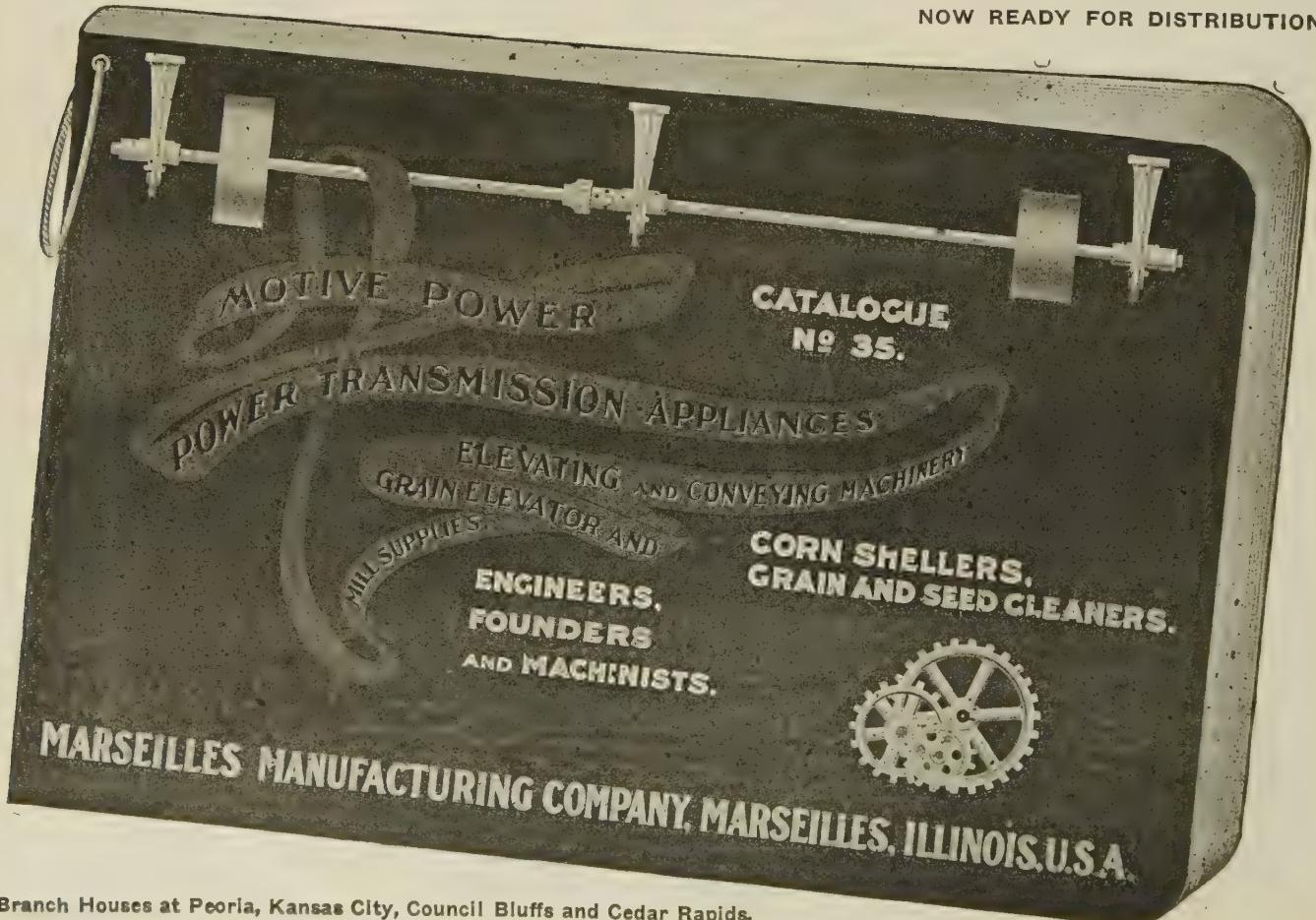
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MILL OWNERS
MUTUAL FIRE INSURANCE CO.
Des Moines, Iowa.
Insures Mills, Elevators, Warehouses and
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in America. Saved to Members
nearly \$1,000,000.
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Reliable Insurance...

on Modern elevators and Contents can be
secured at about one-half the rates
charged by stock companies by addressing
MILLERS NATIONAL INS. CO.
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CHARTERED, 1865 ASSETS, \$2,721,893
NET CASH SURPLUS, \$469,382.27
W. L. Barnum, Secy.

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JAN. 1, 1902.
Gross Premium Notes.....\$697,501.33
Surplus to Policy Holders.....697,351.55
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MILLS AND ELEVATORS ONLY PURELY MUTUAL

A liberal policy issued.
Losses paid when adjusted and NO DIS-
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Michigan Millers Mutual Fire Insurance Co. of Lansing, Michigan.

21 Years Successful Business.

Assets.....\$958,473.31
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Net Cash Surplus, 214,743.50

50% DIVIDENDS 1899 1900 1901

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FLOATER GRAIN INSURANCE
Special attention to Open Floater Policies
in the best Stock Companies.
Insurance follows grain up and down as the
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ALWAYS have insurance where you have grain.
Simple, Sure, Economical. Investigate,
and you will find it absolute protection and cheap.
Business handled anywhere. Write us.
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25 years' experience. Best of references.

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is dried and made merchantable in the

Hess Pneumatic Grain Drier

cheaper and faster than in any other device.

It dries other grains just as well.

Hess Warming & Ventilating Co.
707 Tacoma Building
CHICAGO

THE PERFECTION GRAIN DRIER

IS THE SIMPLEST
AND BEST

Its first cost and cost of operation is 50% less than any other drier. Perfect work guaranteed. Write for particulars.

TWEEDALE & HARVEY
303 Dearborn Street
CHICAGO

Gas Engine Books

Operators of gasoline engines who encounter difficulties in the care or operation of gas and gasoline engines will find each of the following books of great assistance.

THE PRACTICAL GAS ENGINEER,
by E. W. Longanecker, M. D., Price, \$1.00.

THE GAS AND GASOLINE ENGINE,
by Norman & Hubbard, Price, \$1.00.

THE GAS ENGINE HANDBOOK,
by E. W. Roberts, Price, \$1.50.

For any of the above address,

Grain Dealers Co.,
255 La Salle St. Chicago, Ill.

FOR
STRENGTH
DURABILITY &
ECONOMY

CARTON CONVEYOR BELTS

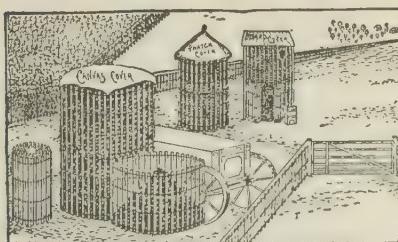
Have no equal for Conveying or power transmission.

Catalogue and prices for the asking.

The Carton Belting Company,
Allston District,
Boston, Mass.

Stronger and cheaper than leather or rubber.

PORTABLE CORN CRIBS

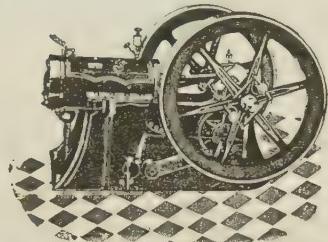


The demand for CORN CRIBS will be enormous this season. Farmers are preparing to build new ones now. YOU SHOULD STOP THEM IMMEDIATELY by offering our TELESCOPE CRIBS, that are better, and at a fraction of what theirs will cost. Capacity: 125; 200; 300; 500; 800; 1,200 bushels. Order printed matter and dozen cribs now. Price will be right.

THE ELLIOTT & REID CO., Box 2060, Richmond, Ind., U. S. A.

POWER FOR GRAIN ELEVATORS.

Power for Every Class of Work.



The Waterloo Gasoline Engine Co., Waterloo, Ia.
Branch Houses: David Bradley & Co., Council Bluffs, Iowa; Bradley, Clark & Co., Minneapolis, Minn.

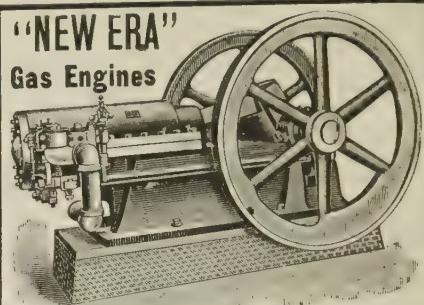
A CARD LIKE THIS

Would do YOU a great deal of good at a small cost.

SOLD ENGINE THRU AD.

John H. Doyle, Longview, Ill.: "Please discontinue my advertisement in the Grain Dealers Journal, as I have sold my engine thru the ad., and could have sold one-half dozen engines if I had them."

"NEW ERA"
Gas Engines



For Gas or Gasoline.

Sizes 5 to 80 H.P.

NEW ERA IRON WORKS,
86 Dale Avenue, DAYTON, OHIO, U.S.A.

HOWE SCALES AND HOWE ENGINES

Can you get anything better?

Can you get anything as good?

INVESTIGATION WILL DETERMINE.

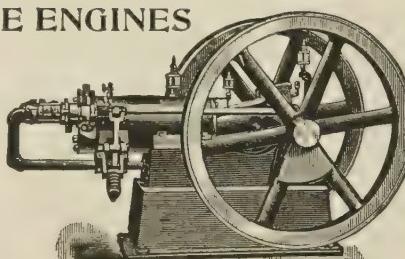
Grain Testers, Grain Scoops, Bag Holders, Car Starters, Conveying and Elevating Machinery.

CATALOGS.

The Ball-Bearing Scale.

St. Louis, Kansas City,
Minneapolis, Cleveland.

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1 to 60 H.P.

Wagon, Dump, Hopper,
and Grain Scales.Double or Compound Beams.
Steel Frames.Grain Receiving
Ledger FORM 33.

Is designed for use by grain buyers who keep individual accounts and is ruled for facts regarding wagon loads received from farmers. Each book contains 200 pages and each page is ruled for records of 39 wagon loads. The pages are numbered and a good index is bound in front part of book. Each page can be used for one or more accounts as desired. The pages are 8x13 inches and ruled with column headings as follows: Date, Article, Gross, Tare, Net, Bushels and Pounds, Price, Debit, Credit and Remarks. The paper used is heavy linen ledger, bound in heavy cloth covers with Russia back and corners. Price \$2.25.

FOR SALE BY

GRAIN DEALERS COMPANY
255 LaSalle St., CHICAGO, ILL.

AN AD AND THE RESULTS.

HIGH GRADE Buckeye Engine for sale; in first-class condition; cylinder 10x14; 2 drive wheels 60-in. diam., 4-in. steam pipe; will develop 75 h.p. A. Colvert, Attica Ind.

Attica, Ind., Sept. 9, 1901.

GRAIN DEALERS JOURNAL,
Chicago, Ill.

Gentlemen: Enclosed please find check for advertising engine. Kindly discontinue ad in Grain Dealers Journal as it has already sold my engine, and oblige,

Yours very truly,

A. COLVERT.

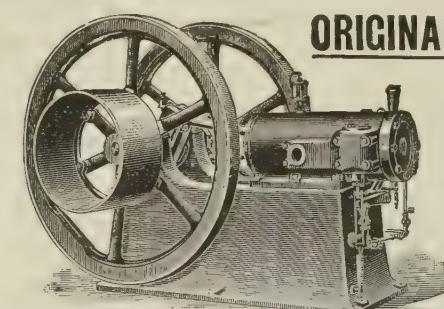
GRAIN
CONTRACT
BOOK

This book is designed especially for country grain men to use in taking written contracts from farmers. The contracts are numbered in duplicate, two on a page, with perforation between for easily tearing apart. The stub is signed by farmer certifying that he has sold bushels of at per bushel to be delivered on or before It also certifies that he has received dollars on the contract. The other part is signed by elevator man and given to farmer. It certifies that the elevator man has not so much grain, etc.

Each book contains 50 contracts, printed on linen paper, size 3 1/4 x 10 1/4 inches.

You can not afford to be without these contracts as they insure delivery of grain bot, prevent losses and worry in making settlements. Book No. 9, Price 50 cents. Address

Grain Dealers Co.
10 Pacific Ave. Chicago, Ill.



ORIGINAL
LEWIS
GASOLINE ENGINES

Most Popular and Reliable for
GRAIN ELEVATORS
WATER WORKS
ELECTRIC PLANTS

Don't be bothered with a cheap, inferior engine. It will always make you trouble and be the most expensive in the end.

Buy a Lewis at the start. It is acknowledged to be standard in all respects.

J. THOMPSON & SONS MFG. CO., Beloit, Wis.

MODERN POWER TRANSMISSION ENGINEERING

exemplified with typical installations in each issue of

Power and Transmission

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Of special interest to those contemplating mill or factory improvements.

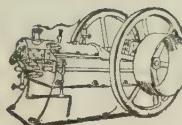
Sample copy free. Address.

THE DODGE MANUFACTURING CO., MISHAWAKA, IND., U.S.A.
MANUFACTURING ENGINEERS.

BRANCHES — Boston; New York; Chicago; Cincinnati; Atlanta; London, Eng.

POWER FOR GRAIN ELEVATORS.

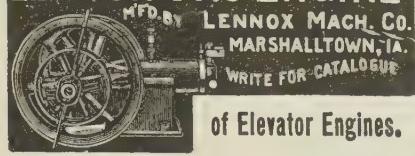
THE BAUER GASOLINE ENGINE



Is better adapted to the needs of the grain elevator man than any other.

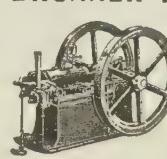
WRITE FOR DESCRIPTION.
Bauer Machine Works,
Kansas City, Mo.

LENNOX GAS ENGINE



of Elevator Engines.

BRUNNER ELEVATOR ENGINE



FOR GRAIN ELEVATORS.

From 1 to 30 H.P.

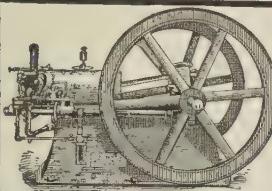
Write for descriptive circular.
CHARLES BRUNNER, Mfr.,
Peru, Ill.

Burger Automatic

Gasoline Engines are Perfect

In mechanical construction, so when buying one for your elevator don't stop short of the best. Write us today.

WOOLLEY FOUNDRY AND MACHINE CO.
ANDERSON, IND.



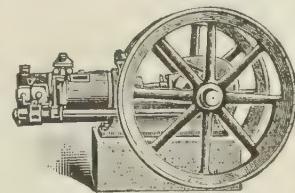
Gasoline Engines

Especially adapted for elevator use.
Fremont Foundry & Machine Co., Fremont, Neb.

IN these progressive days, every merchant, miller and factory owner should have his own isolated electric light plant. The electric light is handy, safe and economical, especially where OTTO Gas and Gasoline Engines are used as a prime mover. We have had the Experience in building special electric engines that insures the purchaser a close regulating and economical motive power. Tell us your needs and we will submit a proposition.

THE OTTO GAS ENGINE WORKS

Chicago Representative, T. W. SNOW, 360 Dearborn St.



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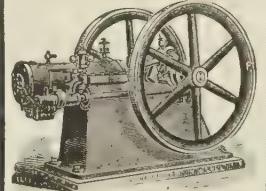
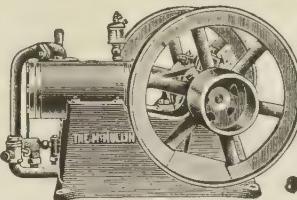
THE CHEAPEST POWER PLANT ON EARTH

is the Gasoline Engine. Learn something to your advantage about

McMullin Engines

by writing us. Catalog and prices on application.

McMullin Motive-Power and Construction Co.
404 Royal Insurance Bldg., CHICAGO.



THE Columbus

Gas and Gasoline Engines.
Simple, effective, easily started and adjusted.
Columbus Machine Co., COLUMBUS, OHIO.
Send for Catalog No. 39



Help!

HELP! HELP!

If you want help that has had experience in the grain business, want to find some one who wants such help, or if you want a partner to help you, make your want known to the trade through the "Wanted" columns of the Grain Dealers Journal, Chicago. It is the grain trade's accepted medium for "Wanted" and "For Sale" ads.



Polk's Directory

of the entire
Grain, Seeds, Elevator, Milling and Baking

Interests of the United States

Addresses of all Grain Commission Men, Grain Shippers, Dealers, Buyers, Elevators (with power and capacities), Grain Machinery, Supplies, and all interests pertaining to the Grain Trade.

SECOND EDITION NOW IN COURSE OF PREPARATION.

R. L. POLK & CO., 122 La Salle Street,
CHICAGO, ILL., U. S. A.

OFFICES IN FOURTEEN PRINCIPAL CITIES.

ORDERS RECEIVED AT GRAIN DEALERS JOURNAL OFFICE.

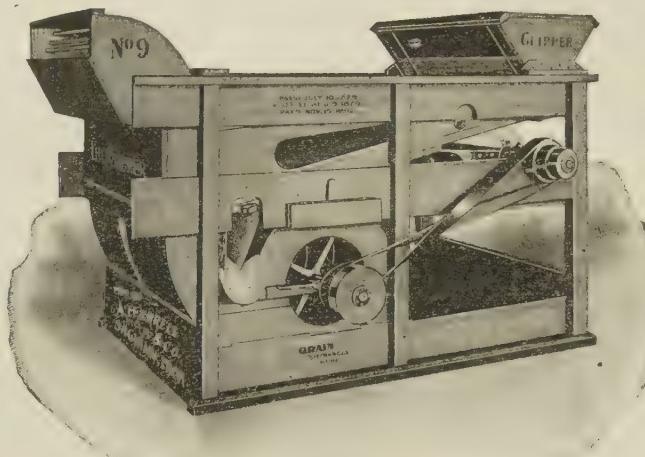
White's Wheat Flakes

Is the most palatable cereal food on the market. A trial will convince you.
If your grocer does not keep it, write

T. G. WHITE, Miller,

Marion, Iowa.

The Clipper Corn Cleaner



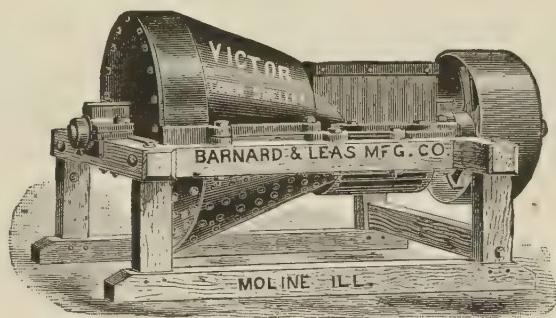
No. 9 Regular Clipper Cleaner.

A. T. Ferrell & Co., Saginaw, Mich.

This Cleaner is well adapted to handling all varieties of either field or sweet corn. The cone pulleys are so arranged as to give an extra strong air blast, which carries out all chaffy, shrunken kernels, husks, silks, pieces of cob, etc. The perforated Zinc Screens are selected to take out all the "bullhead" and "tip" grains if desired, leaving a very fine grade of corn.

This machine requires less than two horse power for cleaning corn or grain and has no equal for elevators of medium capacity. The Clipper Cleaner is easy to install, simple to operate and gives the very best results.

Write for our Sample Plate of screen perforations and catalogue.



THE VICTOR CORN-SHELTER

The oldest, most durable and best corn-sheller on the market. More of them in use than any other sheller. Possesses unequaled capacity, strength and durability.

The Cornwall Corn-Cleaner

A companion machine to the Victor Sheller. The best known and most efficient of the shaker cleaners. It is provided with our patent sieves which are the only sieves that will not clog.

With the Victor Sheller to shell your corn, and the Cornwall Cleaner to clean it, you have an outfit that cannot be beat.

We also make Feed Mills, the Perfected Separator, and a full line of Scourers, and Oat Clippers.

We make or furnish everything needed in Warehouses and Elevators.

BARNARD & LEAS MFG. CO. - MOLINE, ILL.
BUILDERS OF ELEVATORS AND ELEVATOR MACHINERY.

Purify Grain Before Shipment.

WE can purify oats, corn, barley, wheat, etc., and remove mold, must, smut, unnatural odors, insect life and fungus growth.

We can greatly improve the condition and grade of the worst quality of oats, even when containing rotten grains or bin burned.

We can take No. 4 and off-grade oats and restore to condition as bright and sweet as Standard or No. 3 oats, and we can take No. 3 oats and make as bright and sweet as No. 2.

We can remove all sour smell from hot corn, and greatly improve its condition at any stage of deterioration.

We can purify all grains with very little or absolutely no heat, so that grain is ready for shipment as soon as purified, thus requiring no additional machinery or extensive manipulating to cool the grain before shipment. This result of purifying without superheating the grain has never been accomplished and is not possible under any other of the many patents on grain purifiers now on the market.

By our Process, after removing all unnatural odors and smells, we leave unimpaired the natural aroma of the grain—a result never before accomplished.

We can dry moist grain and cool warm grain by throwing as much pure air in contact with it during one elevation as could be brought in contact with it by elevating twenty times to cupola and dropping to basement.

We can accomplish all the foregoing points and results at an EXPENSE LESS THAN ONE DOLLAR PER THOUSAND BUSHELS OF GRAIN, by the use of the

CALDWELL-BARR GRAIN PURIFIER

AND THE

CALDWELL-BARR PROCESS FOR PURIFYING GRAIN

which Purifier and Process are both fully covered by our several U. S. Letters Patents applications.

Grain made as ghastly white as a corpse, with odor suggesting the orthodox Hades, formerly went to market and commanded a premium; to-day such grain meets with a discount. If you desire further proof of this go to the sample tables on the grain exchange of any one of our great cities.

The trade is now educated, and while insisting on grain free from rust, smut, mold, odor and stain, it also positively demands that the grain shall have the NATURAL COLOR and NATURAL AROMA.

If you intend to purify and condition your grain before shipment, always keep in mind and do not for a moment lose sight of what should be done, aye, what MUST be accomplished if you desire a profit rather than a loss, and what MUST be done is comprehended in the six following propositions:

1st—Remove unnatural colors and climatic stains.

2nd—Retain the natural color of the grain.

3rd—Dispel unnatural odors and smells.

4th—Retain the natural aroma of the grain.

5th—Expel the agents by which the foregoing are accomplished, and do it promptly—not weeks later, but immediately.

6th—Leave the grain with its food value and strength undiminished and its qualities for reproduction as seed unimpaired.

Any process or device that will not accomplish the above six results is not worth having, and instead of giving you profit and comfort will bring you annoyance and chagrin, and in this day on your best markets, loss as well.

If you are interested in these matters write us and we will furnish you descriptive pamphlets and samples of grain before and after purification. Or if you wish to see the work being done, you can do so by visiting our elevator at Earl Park, Indiana, 96 miles from Chicago, on the C., C., C. & St. L. (Big Four) Railroad. Address

CALDWELL & BARR, Earl Park, Indiana.

We can save you the cost of Purifier and Process in a short time on handling grain. We can sell you the right to use this Process, and furnish you at a small cost the necessary mechanical parts of the Purifier, and your own engineer can readily construct it, or we can furnish you a mechanic to put it up. Our Process can be made use of by elevators operated by gasoline, electricity or horse power as well as elevators operated by steam.

GRAIN DEALERS' EXCHANGE.

The rate for Advertisements in this department is 15 cents per line each insertion.

ELEVATORS FOR SALE.

ELEVATORS for sale or trade for farms. W. A. Thompson, Attica, Ind.

LARGE country elevator for sale. Well situated. For information write to Walker & Snell, Walker, Ill.

ILLINOIS elevator at sacrifice or exchange for real estate; must be sold. W. L. Cadle, 440 Canal-st., Chicago.

SELL or trade 1-3 or all new 30,000-bu. elevator in Ind. R. S., box 8, care Grain Dealers Journal, Chicago.

ELEVATOR in good condition, for sale. Capacity, 10,000 bu.; gasoline engine. C. W. Henrichs, Bunker Hill, Ill.

STEEL STORAGE TANK, 55,000 bu. capacity, to lease, with track, track scale and elevator privileges. Address T. G. White, Cedar Rapids, Ia.

DWELLING, 10 acres, store, cribs, scales. Purchaser can buy grain on commission and manage elevator on R. R. ground. Frank Hart, Sinclair, Ill.

ELEVATOR in one of the best grain counties in Kansas on C. R. I & P.; no competition; new engine, new feed grinder. H. B. Dickerhoof, Severance, Kan.

A GOOD elevator, and hay, coal, feed and flour trade in connection. Crop prospects never better. Good reasons for selling. Address A, box 2, care Grain Dealers Journal, Chicago.

ELEVATOR and corn meal mills for sale in Deepwater, Mo. Heavy crops; good business. Reason for selling, old age. Price \$4,000. Address Deepwater Real Estate & Exchange Co., Deepwater, Mo.

ELEVATOR and mill on the Lake Shore Road in Ohio for sale. Ship 400 cars annually; no grain dealer here now. Storage, 15,000 bu. Plant worth \$13,000. Address Miller, 72 Traders bldg., Chicago.

GRAIN elevator at Yellow Springs, Ohio, town 1,800 inhabitants; no opposition; in good grain country. All in running order, with established business. For particulars address Meda Brooks, Yellow Springs, Ohio.

CENTRAL INDIANA elevator for sale at a bargain; capacity, 15,000 bu.; in a good grain county; 150,000 to 200,000 shipments annually. Good reasons for wanting to sell. Terms reasonable. Address Box 46, Cicero, Ind.

ELEVATOR and 360 frontage on side track at Mt. Carroll, Ill., for sale to close estate. Capacity 40,000 bu.; gasoline engine and dump. Good opening for right man. For particulars inquire of N. H. Halderman, Mt. Carroll, Ill.

ELEVATOR in good grain town in western Iowa; 25,000 bu. capacity; good coal business; crib room for 50,000 bu. ear corn. Will sell or trade for good farm property. G. H. S., box 5, care Grain Dealers Journal, Chicago.

ELEVATOR for sale in the heart of the wheat belt of Oklahoma; 12-h.p. Fairbanks Gasoline Engine; No. 3 Invincible Combination Cleaner; a 100-bu. hopper scales; all in first-class repair. Elevator 12,000 bu. capacity, with good corn crib in connection. Good wheat crop and good corn crop every year; \$2,500 buys it. S. T., box 6, care Grain Dealers Journal, Chicago.

ELEVATORS FOR SALE.

TWO GOOD elevators for sale, situated on B. & M. R. R. Price, \$6,000. Fine crop this year. Address F. C. Kellogg, McCook, Neb.

ACCOUNT OF HEALTH will sell or trade my elevator, coal and feed business, situated in a never failing crop portion of Iowa. Address E., box 6, care Grain Dealers Journal, Chicago.

ELEVATOR for sale; 25,000 bu. capacity; cribs, 15,000 bu.; coal house; good point; 250,000 bu. annually. Price, \$8,500; easy terms. Address L. S., box 8, care Grain Dealers Journal, Chicago.

ONE mill and elevator and four elevators for sale in Indiana, two in Iowa, one in eastern Illinois with hardware store and implements in connection. Some rare bargains and money-makers if taken soon. List your elevators for sale with me. Aaron Smick, Decatur, Ill.

PAYING LINE of elevators in first-class agricultural territory of Illinois. Yearly business of corn, wheat and oats about 1,000,000 bu. Properties located at Stonington, Willeys, Taylorville, Clarksdale, Raymond and Honey Bend, total storage 303,000 bu. Present owner and manager has had these properties many years, always profitable. Nothing better can be found. Every condition, including competition, is good. We offer the entire business at \$35,000; prefer to sell all together. For additional information address the Pratt-Baxter Grain Co., Taylorville, Ill.

GRAIN WANTED.

BUCKWHEAT GRAIN WANTED. Address H. H. Emminga, Golden, Ill.

WANTED: Feed barley, and new No. 2 and No. 3 rye. W. H. Small & Co., Evansville, Ind.

BUCKWHEAT wanted, by the car; dry and ready to grind. Quote prices. Patterson & Skinner, Pine River, Wis.

EAR CORN wanted: 150 cars delivered about Nov. 1 at Holland, Tex., on M. K. & T. R. R. Who will submit prices? W. L. Crosthwait, M. D., Holland, Tex.

GRAIN FOR SALE.

WHITE WHEAT AND UTAH ALFALFA SEED. When needing white wheat or choice Utah alfalfa seed, write Sam Williamson, Salt Lake City, Utah. Address telegrams "Williamson," Salt Lake City, Utah.

MILLS FOR SALE.

BEST 100-bbl. mill that money could build; 30,000 bu. storage; everything 1st-class shape; large corn crop to handle; best farming section of Missouri. Handled 90 cars of wheat this season. Will sell at big bargain if sold at once. Address Ed. S. Harte, Knob Noster, Mo.

75-BBL. Ohio mill with 10,000-bu. capacity elevator for sale. Practically new and running steady with a large crop of corn to handle this fall. Owner wishes to quit business. Will sell less than cost and very reasonable terms. Write me when you mean business. L. H. G., box 4, care Grain Dealers Journal, Chicago.

ELEVATORS WANTED.

WANT TO BUY an elevator at good grain point in eastern Neb. or southwestern Iowa. J. W. Bailey, Brock, Neb.

GOOD IOWA elevator wanted. Must handle 150,000 bu. or more per annum. Address B. C., box 7, care Grain Dealers Journal, Chicago.

WANTED TO BUY—A line of five or more country elevators located in a territory in Minnesota tributary to Minneapolis. Give full particulars and price first letter. W. H., box 7, care Grain Dealers Journal.

WANT to buy a good elevator in good territory; eastern Ind. or western Ohio preferred. (Prefer corn and oats territory.) Must be in good town and handle not less than 100,000 to 250,000 bushels per year. Address E. B., box 1, care Grain Dealers Journal, Chicago.

SCALES FOR SALE.

SCALES, 2d-hand, all sizes, also new ones cheap. Chicago Scale Co., Chicago.

SIXTY-bu. hopper scale for sale. A. F. Chase & Co., 215 3d-st., So. Minneapolis, Minn.

HOPPER SCALE for sale, 60-bushel Fairbanks. C. D. Holbrook & Co., Minneapolis, Minn.

HOPPER scale for sale: a 200-bu. Fairbanks; used three months. Address E. A. Thorp, Garber, Okla.

PARTNERS WANTED.

PARTNER wanted, with \$6,000, to engage in the grain business. Address Illinois, box 8, care Grain Dealers Journal, Chicago.

WANTED Practical grain man or implement man to buy one-third to one-half stock in \$30,000 incorporated grain, lumber and implement business in best farming, mining and manufacturing town in Wisconsin. Profits 30 per cent net annually on investment. Sickness reason for selling. Only business men need apply. Address Wisconsin, box 8, care Grain Dealers Journal, Chicago.

ONE of the best milling and grain plants in Kansas wants a first-class grain man to take an interest in the business and manage the grain department. If you have \$5,000 to invest you are offered a rare opportunity to establish yourself in a permanent and paying business. Prefer a man who can speak Swede. If you don't mean business don't answer. Address M. G., box 8, care Grain Dealers Journal, Chicago.

MISCELLANEOUS.

THE Grainman's Actuary \$1.00 postpaid. Henry Nobbe, Farmersville, Ill.

CORN CRIB VENTILATORS. Adjustable; fit any crib. Satisfaction guaranteed or no pay. For full particulars write N. S. Beale, Tama, Ia.

C. A. BURKS
GRAIN ELEVATOR BROKER
MERCHANTS EXCHANGE.
DECATUR, ILL.

GRAIN DEALERS' EXCHANGE.

The rate for Advertisements in this department is 15 cents per line each insertion.

ENGINES FOR SALE.

GASOLINE ENGINE of 3½ h.p. for sale. W. B. Murray, 1253 Milwaukee-av., Chicago.

SECOND-HAND engines for sale: size one 14x36 and one 18x42. A. F. Chase & Co., Minneapolis, Minn.

BOILER and engine for sale cheap and in good condition; 25-h.p. Box 194, Portage la Prairie, Manitoba.

NEW ERA 26-h.p. Gas Engine for sale. Good as new. Geo. Middendorf & Co., 135 S. Water-st., Chicago.

NEW ERA 16-h. p. Gasoline Engine for sale; also elevator belt and cups. G. H. Warren, Middletown, Ill.

SECOND-HAND 15-h.p. Standard Oil City Gas Engine for sale; \$300. Write for other sizes. Lammert & Mann, 155-161 S. Jefferson-st., Chicago.

ONE second-hand tubular boiler, 16 ft. by 52 in.; 20-ft. smokestack, 24 in. diam. Price, \$100, f. o. b. cars, Kempston. M. J. Lee & Son, Kempston, Ind.

ONE first-class gasoline engine, 25 h.p., in the best of condition, for sale. Used only about four months. Address Geo. L. Brown & Son, 222 American Bank bldg., Kansas City, Mo.

ONE 16, one 12 and one 6-h.p. Fairbanks, one 13 and one 5-h.p. Otto Gasoline Engines, latest style, for sale or exchange; also other makes. A. H. McDonald, 36 W. Randolph-st., Chicago.

CENTER CRANK ENGINE for sale; 14x16; manufactured by the Erie City Iron Works; rated at 70 h.p.; used about 4 years; in first-class condition. Inquire Iron Elevator & Transfer Co., Buffalo, N.Y.

SECONDHAND GASOLINE ENGINES: One 22-h.p. Fairbanks, \$500.00; one 12-h.p. Olds, \$300.00; one 3-h.p. Oline, \$100.00; one 2½-h.p. Weber, \$100.00; one 1½-h.p. Webster, \$85.00. For sale by Allen P Ely & Co., 1110 Doug.-st., Omaha, Neb.

STEAM AND GASOLINE engines for sale; 26, 12 and two 3-h.p. Charter; 17-h.p. New Era; 4-h.p. Racine, and 5-h.p. Fairbanks Gasoline Engines. Steam engines of 15, 35 and 40 h.p.; and boilers of 20, 40 and 50 h.p. C. D. Holbrook & Co., Minneapolis, Minn.

FOR SALE—Second-hand gasoline engines, 1 to 50 h.p. Why buy new engines when we sell slightly used and guaranteed at one-half original cost? We have all makes and all sizes. Write us, stating your needs. Price Machinery Co., 507 Great Northern bldg., Chicago, Ill.

ONE 4-h.p. Otto; one 10-h.p. Otto; one 12-h.p. Lambert; one 22-h.p. Fairbanks; one 54-h.p. Fairbanks; one 20-h.p. New Era; one 26-h.p. New Era; one 35-h.p. New Era; one 60-h.p. New Era. I buy, sell or exchange. J. Montgomery Johnson, 216 Lake-st., Chicago, Ill.

ONE 12-h.p. Ideal Balance Valve Horizontal Steam Engine, 7 in. bore, 10 in. stroke, weight 1,800 lbs., complete with lubricators. Price, \$140. One h.p. horizontal steam engine, center crank, 6 in. bore, 8 in. stroke, weight 700 lbs., complete with lubricators. Price, \$100. J. Thompson & Sons Mfg. Co., Beloit, Wis.

ENGINES FOR SALE.

FOR SALE:—Boilers & engines all sizes, \$35.00 up. The Bicknell Hdw. Co., Janesville, Wis.

OTTO 10-h.p. Gas Engine, in good condition, for sale. Address Parkside Mfg. Co., 86 E. Ohio-st., Chicago.

CHEAP: An almost new Otto 5-h. p. engine. Guarantee it to be as good as new in every respect. We bought 15-h. p. Write Revere Elevator Co., Revere, Mass.

MACHINES FOR SALE.

A 4 or 5-horse feed mill, in good condition, for sale cheap. E. E. Noe, Eldora, Ia.

SECOND-HAND CLIPPER MILLS for sale. Apply to the Albert Dickinson Co., Chicago, Ill.

WILLFORD 3-roller mills for sale; one No. 1, one No. 2. C. D. Holbrook & Co., Minneapolis, Minn.

A CORN, oats and wheat cleaner of large capacity; a bargain; 10 days' trial. B. S. Constant Co., Bloomington, Ill.

CORN cleaner, Barnard & Leas, No. 2, double cylinder, good condition. Will sell cheap. I. R. Kennard, Moran, Ind.

ONE Eureka No. 5 Oat Clipper and one Barnard & Leas No. 1 Elevator Separator. Bartlett, Kuhn & Co., Evansville, Ind.

PIONEER Wagon Dump for sale at 2-3 cost, f. o. b. here; in good shape; ready to run. W. P. Finger & Sons, Sawyer, Kan.

ONE No. 1 Barnard & Leas Separator, used but a short time, all in good repair, \$65 f.o.b. East Prairie, Mo. Address E. D. Russell, East Prairie, Mo.

AN ELEVATOR OUTFIT consisting of cast iron shoe, pulleys, 4½x7 cups, and 90-ft. rubber belting, 8 in. wide, for cups, all in good condition, for sale. D. Miller, Lanark, Ill.

SEND for our list of new and second-hand shellers, cleaners and meal and feed grinding machinery, mill and elevator supplies. The E. E. Hollister Co., 116 North Front-st., Quincy, Ill.

TWO THOUSAND-BUSHEL "Three Rivers" Corn Sheller, run one season. Pulleys, shafting, belting and forty-foot elevator complete. Will sell at a bargain. McClure Bros., Dallas, Texas.

SECONDHAND machines for sale cheap: 1 No. 0 Cornwall Corn Cleaner; 1 No. 0 Victor Corn Sheller. Also a number of other secondhand machines. Address R. C. Stone, Springfield, Mo.

CORN CLEANER for sale cheap. Rolling screen corn cleaner, Smith make; in good condition; capacity 300 to 400 bu per hour. Larger cleaner put in reason for selling. W. E. Tuttle, Springfield, O.

ONE No. 3 Miami Valley Corn Cleaner; 1 Smith Patent Ear Corn Feeder; 1 Eureka Wheat Cleaner, brand new; was never set up, for sale. Price is right. Raymond P. Lipe, 50 St. Clair bldg., Toledo, O.

ABOUT 150-ft. Jeffrey Chain Conveyor, suitable for an ear corn drag or for conveying small grain either from dump or from storage bins. For further particulars and price write to Churchill & Co., Buffalo, N. Y.

MACHINES FOR SALE.

METCALF Bifurcated Car-Loaders for sale; bargain, secondhand. T. V., box 1, care Grain Dealers Journal, Chicago.

TWO No. 4 Eureka Clippers; good condition. Sell because I have not power enough to run. D. N. Dunlap, Fontanelle, Iowa.

A No. 4½ Combined Western Sheller & Cleaner, and a No. 7 Clipper Cleaner, for sale; both replaced by larger machines. Gordon & Dehring, Curtice, O.

ONE No. 2 Marseilles Corn Sheller; one No. 1 Three River Corn Sheller; good as new; for sale or exchange for engine and boiler. Address Schiff & Weathers, Greenville, Tex.

OAT CLIPPER for sale at low price, as we have no use for it. Machine does very good work and can be run at 200 to 1,000 bu. per hour, requiring 5 to 25 h.p. Best made. Address J. B., box 7, care Grain Dealers Journal, Chicago.

LARGE BARLEY AND WHEAT REEL, about good as new; 60 reels, an exceptionally fine machine for taking out all kinds of seeds and small kernels. Cost \$400. Will take \$100 cash. S. D. H., box 4, care Grain Dealers Journal.

ONE No. 8, 4 No. 5 and 5 No. 4 Knickerbocker Cyclone Dust Collectors; No. 9 Monitor Oat Clipper; 2 No. 9 Monitor Warehouse Receiving Separators; 2 Twin City Succotash Machines, 100 to 250 bu.; 2 flax machines, 100 to 150 bu.; 4 friction clutch pulleys, 48x8; 3 36-in. and 1 76-in. 5 gr. friction clutch sheaves, 1-in. rope. G. T. Honstain, 313 3d st., S., Minneapolis, Minn.

ONE 24-in. attrition mill; one No. 6 oat clipper; one No. 3 receiving separator, with 2 sets sieves, wheat and corn; one new Howe Hopper Scale, 300 bu. capacity hopper, ironwork and beam strong enough for 500 bu., everything new, timbers and hopper go with it. One, 9x18, 3-pair-high roller mill, new, John T. Noye make. Above machines are as good as new and will be sold cheap. Prices on application. A. S. Garman & Sons, Akron, O.

ENGINES WANTED

SIX to eight horse gasoline engine wanted; also small corn grinder and meal outfit. W. F. Starz & Co., Fowler, Ind.

HELP WANTED.

EXPERIENCED grain man with sufficient tact to travel and represent effectively the interest of two growing local associations. Salary \$800 to \$1,000 and traveling expenses. Address DISCREET, Box 8, Grain Dealers Journal, Chicago, Ill.

SITUATIONS WANTED.

POSITION: Would accept with good party to buy and manage grain business; 18 yrs. experience, last 12 as manager. Reference given. Address R. O., box 7, care Grain Dealers Journal, Chicago.

POSITION wanted by a man that thoroughly understands erection of country elevators including both steam and gasoline engines and setting all kinds grain cleaning machinery. X.Y.Z., box 8, care Grain Dealers Journal, Chicago.

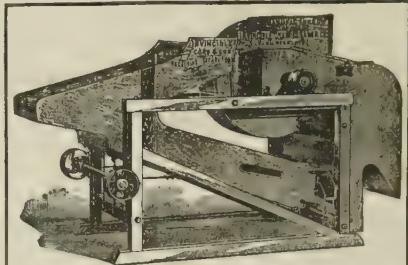
STAINED OATS AND BARLEY

can be made bright and sweet by our patent process of purifying. Then, too, you can get a better price for your grain. The impurities such as smut, must and mold odors are entirely removed.

A purifier can increase your profits enough to pay for itself in a short time. . . . Write for particulars to

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THE INVINCIBLE CORN AND COB SEPARATOR & CLEANER



IT cleans corn and separates the cobs from the shelled corn, as it comes from the sheller, with one operation. It is durable, light running and dustless. Your corn will never grade dirty when this machine is used. This machine is fully guaranteed and shipped to responsible parties on thirty days' trial.

We manufacture Oat Clippers, Receiving Separators, Scourers and Cleaners, Needle Screen Gravity Separators and Spiral Belt Separators. . . . SEND FOR CATALOG

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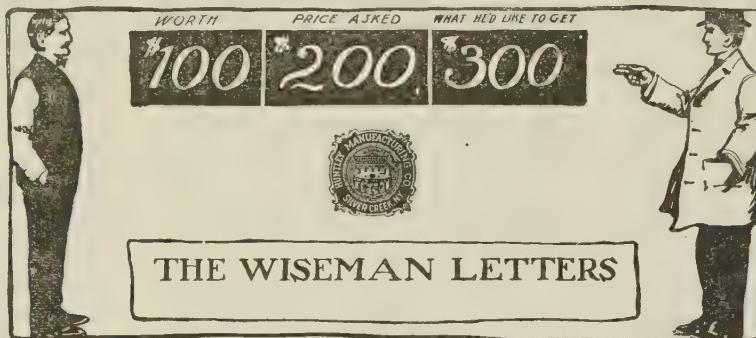
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Prompt Shipments Guaranteed.

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Steam Engines and Boilers

Pulleys, Shafting and Hangers.
Rubber, Leather and Canvas Stitched **BELTING.**



CUTTING PRICES.

My Dear John:—

Just got your letter in which you tell me about the man from Just-as-Good & Co. who offers you a \$300 machine for \$200.

Since the old days I have cut my wisdom teeth several times. It has been a pretty expensive process, but long ago I got next to the fellow who offered me \$300 worth of "A good thing" for 33 1/3% discount.

When it comes down to Grain cleaning machinery it has always been a very interesting question with me to find out just where the manufacturers would stop in the question of price.

In lookin' over the field you will see to-day machines that sold five years ago for \$500 going a-begging for \$300. Long ago I worked out a little problem in this question of price. I figure that your \$300 ma-

chine represents the price your manufacturer WOULD LIKE to get. About \$200 would buy it without any argument, and when it comes down to the fine points, \$100 is about what it was worth.

One thing that I like about the Monitor people is that they have an open-handed way of dealing with you.

There are no loop-holes in the Monitor guarantee and when they give you a guarantee that a machine will do a certain thing, it will do it, and you can get any kind of Grain Cleaning machinery of them that you want. You can equip your entire department with them and every machine is just as good as the other for the purpose that you want it for and every machine is covered by the Monitor guarantee and you get more satisfaction out of every hundred dollars that you put into a Monitor machine than you will in any other make on the market.

Yours truly,

SAMUEL WISEMAN.

THE MONITOR LINE INCLUDES:

MONITOR DUSTLESS WAREHOUSE AND ELEVATOR SEPARATOR.

MONITOR DUSTLESS OAT CLIPPER.

MONITOR DUSTLESS WAREHOUSE SMUTTER.

And a Full Line of Special Grain Cleaning Machinery For Any and All Kinds of Uses.

HUNTLEY MANUFACTURING COMPANY,
SILVER CREEK, N. Y.

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furnished on application. The advertising value of the Grain Dealers Journal as a medium for reaching the grain dealers and elevator men of the country is unquestioned. The character and number of advertisements in its columns tell of its worth.

Letters

on subjects of interest to those engaged in the grain trade, and trade news items are always welcome.

CHICAGO, ILL., OCTOBER 25, 1902.

THE CAR famine is extending everywhere, and no promise of permanent relief is in sight.

CINCINNATI has new rules governing the grading of oats all of which are given in the Ohio column this number.

ST. LOUIS Merchants Exchange certificates are still demanded by shippers to that market who desire correct weight of grain in their cars.

A WEIGHING fee of \$3 per car may seem large, but in cases where this covers the cost of unloading and transferring the grain, it is by no means extortionate.

A NEW feature has been added to the work of the Texas Association which seems to be proving quite valuable to members with old claims on hand. In fact the secretary has succeeded in collecting a number of accounts which had virtually been charged off.

DISCRIMINATION in freight rates has not been entirely eliminated from the trade, but in some sections has been reduced sufficiently to give great encouragement to small dealers who formerly experienced much difficulty in competing with those favored with cut rates.

AGENTS for line companies in Nebraska are beginning to talk about organizing. There is no reason why they should not be able to advance their material interests by discussion of problems which confront them, as well as make their services more valuable to their employers.

BULLDOZING an elevator owner into selling out by unloading a car of stones at his station with the expressed purpose of putting up another elevator, when no such action is intended, is hardly honorable nor honest, and surely cannot be tolerated by any organization believing in fair play.

ORGANIZATION continues to spread to new territory with the prospect that

the Southeast, Missouri, Wisconsin and Michigan will soon begin to ask questions more profusely than ever before. Everyone of these sections merits careful consideration and hard work to get and keep them in line.

THE COAL STRIKE is ended and the railroads will now be pushed to transport coal supplies which should have been distributed during the past five months. This of course will not accelerate the movement of grain and must be taken into consideration by those having grain of doubtful character for shipment.

HAVE you tested the new corn for the amount of moisture contained? Do you know how much it will shrink every day? If not you had better conduct a few experiments before starting to buy. The amount of shrinkage will surprise you. Several reports of tests are published in this number with almost incredible results.

THE MERCHANTS EXCHANGE of Buffalo has an unique though effective way of enforcing the edicts of its arbitration committee, to-wit: "Members are forbidden to deal with an outside firm which refuses to abide by the decision of the reference committee." If arbitration or the decisions of arbitration committees are ever to be made effective, fair-minded dealers must stand by the committees in their decisions.

ELEVATOR men who have their offices in elevator building proper are beginning to fire up with the usual result of fires occurring from mysterious causes. Careful inspection of all stove pipe connections and chimney before starting the fire for the winter will surely reduce the fire hazard and the chances of business being interrupted by destruction of the plant. Losses due to suspension of business are not covered by fire insurance policies.

BEGGING shipments as a reward for not bidding a scooper or disturber is hardly tolerable and in the long run will gain the least reward. The receiver or track buyer who declines to handle the business of the disturbers and continues to work for business in the usual way, will command a much higher degree of respect from those who know of his having cut off the scooper, than if he has begged for shipments as recompense for his cutting off the scoopers.

ILLINOIS politicians are still without control of the weighing of grain at Chicago and Peoria, despite the great need of politicians for more patronage. Chicago had more than its share of trouble with short weights years ago, and at present is well satisfied with Board of Trade weights. In fact the entire trade is of the opinion that Chicago weights are correct. It is sincerely hoped that the trade will not permit the politicians to get control of the Chicago weighing Department.

BUFFALO has taken steps looking to bringing about the organization of a better weighing department to be operated under the supervision of the Merchants Exchange. This surely is a move which will be good news to patrons of that market. Weights have not been entirely satisfactory and any movement for improvement will be more than welcome. If the department is placed under the supervision of the present Chief Grain Inspector, as now seems probable better weights will be assured from the start.

THE DIRECTORS of the Chicago Board of Trade has recently given expression to a sentiment that advertisements pretending to predict the course of the markets, or presenting plans for making money were unbecoming and hence members indulging in this character of advertising would be deemed guilty of dishonorable conduct. It is well that the board has taken such a stand, for this class of advertising heretofore has been indulged in most largely by bucket-shop keepers.

GUARANTEEING weights to within one per cent of the billed weight has long been common practice with Ohio grain shippers, but at the recent meeting of the state association it was decided to guarantee weights to within one-half of one per cent. No man has ever been able to get as much out of a car as was put in unless the load was exposed to moist air and its weight increased by absorption. It would seem proper that the usual shrinkage should be borne by shippers, but if an unusual shrinkage occurs the carrier should be held liable.

ARBITRATION continues to gain favor with the different state and local associations and among cases recently arbitrated and settled out of court are three interesting cases reported in Letters from the Trade in this number by the Secretary of the Texas Grain Dealers Association. The associations are performing for members in such matters services not thought of a few years ago. What is needed is more arbitration and more publicity to the facts involved in the cases. The wider circulation given to facts and decisions, the greater assistance will such work be to other shippers who have similar differences.

STORING grain for farmers at country points is seldom profitable because the elevator operators fear to enforce a fair schedule of storage charges. A South Dakota dealer cited, in this number charges the small sum of two cents per bushel for storing the first fifteen days and $\frac{1}{2}$ cent for each succeeding fifteen days. If revenue rules of this character were always enforced, country elevator men could afford to provide sufficient storage capacity to accommodate all would-be patrons. The great trouble with the storage business always has been that the elevator man did not have sufficient backbone to enforce the collection of charges.

FORGED BILLS of lading do not often result in a receiver's paying for what he does not get, but this does occur often enough to warrant every receiver in exercising extreme care in selecting his shippers. It is not often that the man who has money invested in elevator facilities, will so endanger his freedom, but the man who has nothing invested in the grain business, who is free to pull up and leave on a moment's notice, does not hesitate to forge bills of lading providing he can raise a goodly sum on short notice. Country station agents have always been very careless in permitting anyone to have access to blank bills of lading and no doubt will be, until carriers rigidly enforce strict rules regulating the giving out of bills of lading.

AMBIGUITY in confirmation or acceptance of bids, or sales of grain has often caused needless loss of money, and no doubt will continue to result in trouble and misunderstandings until the trade, out of sheer desperation, gladly adopts uniform forms for all transactions and

thereby reduces the opportunity for misunderstanding. Sometime ago we published a form of confirmation drafted by the Grain Dealers National Association which seemed to meet with the approval of many track buyers. No objection has been given to the form published, but several have commended most highly the plan of securing the adoption of uniform forms. Some of the forms now being used are unfair, others illegal and hence worthless. Uniform forms and uniform practices are not only desirable, but surely are the cheapest, the safest and best.

AN OHIO correspondent in this number calls attention to an omission from our account of the proceedings of the annual meeting of the National Association. The motion omitted was indeed a very important one and should have been published in the last number. The enactment of the law required, however, does not seem necessary. A suit at law would seem all that is needed to fix the liability of the different carriers involved in the transportation of an interstate shipment over different lines. If the initial carrier takes the liberty to route the shipper's grain as suits its own fancy, it is without doubt liable for any damage, which may occur to the shipment at any place between the point of origin and the point of destination. No doubt a law fixing this liability explicitly would remove all doubt and prompt carriers to be more careful in the transportation of freight.

THE CHIEF Grain Inspectors National Association which seeks to establish uniform rules governing the grading of grain throughout the country, adopted at its recent meeting in Peoria rules governing the contract grades of grain with the exception of spring wheat. These rules are published elsewhere in this number. It is the desire of the association that these rules be adopted by the different markets in hope of bringing about uniform grades throughout the land. Uniformity would surely facilitate the grain business of the country, and do away with many opportunities for differences involving grades. If all the markets were to adopt the same rules and then have them enforced with equal carelessness, shippers would soon learn the difference between Nos. 2, 3 and other grades. As it is at present, the grades in the different market vary so greatly that experience generally confuses the shippers. Uniformity would also help our export trade.

CHICAGO grades, or at least the rules governing the grading of corn, are not likely to be changed soon. The sentiment of the dealers attending the meeting of the Illinois Railroad and Warehouse Commission, in Chicago this week did not seem favorable to changing the rules, altho the majority were of the opinion that the department inspectors at Chicago are too rigid in the enforcement of the rules. If the trade insists on having a laxer grading then it would seem advisable, to amend the rules to state explicitly what is desired. If the term "dry" is too exacting and "reasonably dry" is only intended, then, for the protection of the outside trade, as well as the inspectors, the rules should be amended so to state. The opinion that the out inspection is not so rigid as in inspection is gaining ground. It is perfectly natural that the inspection should be more rigid on grain going into a storehouse than that loaded out for shipment away from Chicago, for the reason that if it goes into store in a public warehouse it is likely to be kept there for months, and the inspectors will be

blamed with carelessness if it gets out of condition. While the grain going out of any market soon gets beyond the jurisdiction of the inspectors and is not likely to cause them any further trouble. The account of the hearing before the Illinois Railroad and Warehouse Commission which is published in this number, will prove interesting reading to any one interested in Chicago grades.

THE OHIO Grain Dealers Association is working to induce the Central Freight Association to take steps which shall facilitate and hasten the delivery of new corn. The committee has asked that every car of new corn have large cards tacked on both sides bearing the words "THIS CAR CONTAINS NEW CORN. RUSH." If yardmasters and others who have any voice in making up trains at junction and terminal points were properly instructed in this matter, surely it would greatly reduce the losses forced upon shippers by delay in transit. The tacking of cards to cars; specified in bills of lading that corn is new, rush, and also the issuing of instructions by freight traffic managers to all lieutenants to forward new corn as quickly as possible, can only be brought about by the combined influence of all the associations interested. The move is indeed urgent and merits the hearty support of every association worker. Write your traffic manager to-day.

BUCKET SHOPS are gradually disappearing from some sections of the country and now that the Chamber of Commerce of Minneapolis is making a bitter fight against unlawful institutions of this character in the Northwest it seems certain that fewer of them will be permitted to continue preying on the general public, and bringing all commercial exchanges into disrepute with the misinformed. The uninformed public has so persistently been misled into believing that all boards of trade were illegitimate, all grain brokers were dishonest, simply because the newspapers carelessly published sensational articles regarding the failure of this, that, and the other bucket shop which had been posing as a board of trade, or a member of different exchanges, when in fact it had no connection whatever with any organization except its own ring of robbers. It is almost incredible that the laws of any state should be so lenient toward the sharers as to make it difficult to force them out of business.

KANSAS CITY has a millers club, not for the purpose of reforming the business methods or practices of its members, but presumably for the purpose of reforming other persons' business methods. They would not think of scouring or mixing grain in their mills, and, as to mixing low grade with patent, absolutely impossible. Yet these same millers are much interested in the waywardness of Kansas grain dealers who are given to clipping and mixing wheat, and recently their opposition to such clipping and mixing induced them to instruct their secretary to issue a protest against such practices. The protest is to be beautifully printed and circulated to all the dealers, millers and newspapers of the great southwest. Dealers who receive a copy of this beautifully printed pamphlet will be permitted to smile, file it away in his curio cabinet, and then continue to handle his grain in the way that will bring him the most profit. As long as the millers will pay him more money for clipped and mixed wheat he will not have a very strong impetus to heed the protest.

LETTERS FROM THE TRADE

UNIFORMITY OF CONFIRMATION.

Grain Dealers Journal: I note with interest the editorial comment on "Uniform forms of confirmation." The idea is certainly a good one. If the Grain Dealers National Association could adopt a form of confirmation which would be accepted by the trade in general, being unobjectionable in all features, yet binding to both parties, it would certainly be a step in the right direction.

I certainly hope that the association will take some action in this matter and agree upon a form which will be acceptable not only to shippers, but also the receiver and consumer, protecting the interests of all parties concerned. Yours respectfully, C. A. Burks, Decatur, Ill.

DECISIONS OF TEXAS ARBITRATION COMMITTEE.

Grain Dealers Journal: Thinking possibly a brief statement of some of the important decisions of the arbitration committee of the Texas Grain Dealers Association at its last hearing might be of interest, I give you a brief statement of two or three of these cases without the names of the interested parties. In one case the evidence submitted showed that one party quoted another as follows: "I can quote you one or two cars of bright, new, sound oats, sacked, at 43c per bu. delivered, quick shipment. They are being loaded now." This quotation was made by letter. On receipt of the letter, the other party wired accepting one car, and gave shipping instructions. The party quoting the oats then wrote the other party, stating that he had sold the oats, and could not ship. The party accepting claimed the quotation was made without reserve and he had the right to accept it on receipt of the quotation, as it was not made subject to confirmation. The committee held that the quotation was made without reserve of any kind and was accepted, and rendered a verdict against the shipper of the oats for the difference in the price quoted and the price the purchaser had to pay for oats to fill the order.

In another case, quotations were made subject to a certain elevator weights and grades, and the confirmation of sale also contained that clause. When the car arrived, it was found to be short in the number of sacks and in weight, but the shipper having furnished a certificate of the elevator weights and grades, the committee held that, notwithstanding the grain arrived in the car for which certificates were furnished, the sale having been made on a basis of elevator weights and grades, those weights and grades should prevail, and rendered a verdict in favor of the defendant, who was the shipper of the grain, the plaintiff having made claim against him for the shortage. In considering this case, the committee expressed the opinion that the contract was made on a basis of elevator weights and grades and the evidence showing that the proper certificates were furnished, and if the plaintiff had a claim against any one it was against the railroads.

In another case W. asked D. for quotations on oats delivered. D. wired quotations based on the residence of W. W. wired acceptance and three or four days afterward mailed shipping instructions to D. for the car. In making the shipment D. deducted freight on a basis of the residence of W., as he considered that the basis of his contract. But the oats went

to some place taking a differential rate. There was a small difference in freight and W. made claim against D. for the amount of the difference. The arbitration committee held that D. had no way of telling for what point the oats were desired and held that he had the right to take the residence of W. as a basis and rendered a verdict in favor of D., who was the defendant.—Hugh B. Dorsey, Secy., Weatherford, Tex.

AN OMISSION.

Grain Dealers Journal: I note in your report of the proceedings of the National Association you make no mention whatever of the motion offered by the writer at the request of the Ohio association with reference to fixing the liability for the damage to grain in transit. The motion, which was passed unanimously, is as follows:

"Moved that the matter of fixing the liability for loss or damage to interstate shipments of grain on the initial railroad or transportation company, be referred to the executive committee with the request that if, after investigation, this is deemed a proper matter for national legislation, they will attempt to have a law enacted with reference thereto which shall have uniform application in all the states."

As I understand it the matter will come before the executive committee at its first meeting.—E. W. Seeds, Columbus, O.

[Mr. Seeds is right. The motion was unintentionally omitted.]

TEST OF NEW CORN.

Grain Dealers Journal: On Sept. 27 one of our dealers here put 82 pounds of new ear corn in gas house to dry. Oct. 14 it weighed 62½ pounds. After shelling, weighed as follows: Shelled corn 51 pounds, cobs 11½ pounds. He states this corn is not fit for meal now and will dry out at least 5 or 10 per cent. In fact this stands to reason, for if the corn was dry, 68 pounds of ear corn should not have over 11 to 12 pounds of cobs. We are getting other tests from corn received now and will give record later.

The dealer who cribs corn now and takes less than 90 pounds for a bushel, will have to buy cheap, or lose money, unless corn is in better condition than here. It will be well for every dealer to make tests of this corn, so as to know how to figure in his locality.

One of our friends thinks we made a mistake in giving the kiln dried record. In the first place the time is too short to get a correct record by letting it dry in the usual way, in order to show what cribbed corn will lose. Dealers who crib corn, as a rule, pay too much for the number of pounds they get. If the dealers will refuse to buy corn until after it has been husked and cribbed, they will have less hot corn. In this way every time it is handled the farmer will throw out more or less soft or damaged corn. But when hauled from the field, the dealer gets it all.

In conversation with one of the successful grain men of this state he said: years ago he handled corn early, and it generally took three months to make back losses on the first shipments. He further states that the weather will have to be very favorable if he commences shipping before December 1.

Another successful shipper writes: "We second the motion to let farmers crib their early husked corn and let it dry to be safe for shipment before marketing. With our association the business can be conducted much more satisfactorily to

all parties. Formerly dealers were watchful of each other, fearing the other would get ahead, and each wanting to hold his customer, and afraid to refuse, fearing his competitor would take the wet, green corn and win his customer. But by mutual agreement now, better judgment can prevail, and results will be much better for the farmer and dealer. This year corn will be dangerous, and hot stuff to handle until it has had a freeze and dry weather. Our corn will all need sorting to get out the corn that was milky and soft when it was cut, probably 10 to 20 per cent. Yours truly, E. A. Grubbs Grain Co., Greenville, O.

GRADING OF CORN AT CHICAGO.

Grain Dealers Journal: It seems to be a fact that when our corn market is manipulated and the "cash future" is forced to an unnatural premium over deferred futures, the inspection becomes more "rigid." When everything is natural the lines are not so tightly drawn. The reason for this is, I believe, that the inspector fears his work may be criticised and he sticks to the letter of the rule. He knows that the consignee can ask for re-inspection by his superior, who will change the grade if he has been too severe, and failing here, there is still recourse to the appeals committee. But I know this: When the corn has been once graded No. 3 the first assistant inspector looks for the track (second assistant) inspector's reason for making it No. 3, and can generally find it. In other words, his attention is called to defects that ordinarily would be passed over, and which really should be passed over.

Influential people have approached the chief inspector and his first assistants with the complaint that at present the inspection is too severe, that the grade of No. 2 has been raised until the only corn that is passed as No. 2 is really of such an excellent quality that it should be graded No. 1. The rule giving the requirements of No. 2 corn reads: "No 2 corn shall be dry, reasonably clean," etc. You will note that it does not read "reasonably dry." "Dry" is not modified. I had occasion to make a fight on just this point. In accepting No. 2 corn in fulfillment of contracts, I insisted that the corn must be "dry." The receiving houses made a protest to the Railroad and Warehouse Commissioners, and the inspection department was apparently influenced, as the proportion of contract corn in the daily receipts increased from about 10 per cent to over 50 per cent. Right here I want to say no corn was at that time passed as No. 2 that was ever refused as No. 2 by any one who bought it to ship. It kept in good condition and all went out of store without protest.

With the fear of manipulation ever present, the handlers of corn at country stations everywhere over the corn belt hesitate to "hedge" purchases of new corn with sales of December. It has so reduced the amount of trading in this future as to make it possible for one or a few moneyed men to force the price to a big premium over May. The new corn everywhere will be moved on a basis of the May price, and hedges are and will continue to be made in that future.

This reduces the operations in the December to the purely speculative and gives us an unhealthy condition. There is entirely too much of a gambling nature to such operations, and the best interests of the Board of Trade and of the grain handlers everywhere are not best subserved by permitting this condition to

continue if anything can be done to remedy it.

Thousands of cars of corn every year graded as No. 3 and put into store during the winter months go out in the spring at the same price as No. 2, and fill every requirement of No. 2. This corn is shipped to foreign countries and arrives at its destination in good condition. Consumers here and abroad pay no premium for No. 2 corn. The No. 3 seems to be the commercial grade. This is true, and I believe thousands of cars of corn every winter graded No. 3 could and should be graded No. 2.—Geo. H. Phillips, Chicago.

SHORTAGE RULE AT PITTSBURG.

Grain Dealers Journal: In the issue of Oct. 10 is published an article entitled "Shortage Rule at Pittsburg" in which S. S. Bosselman of LaPorte, Ind., criticises very sharply the treatment accorded him here by one or two dealers on a car of unmerchantable corn.

On behalf of the dealers here I would say that no such rule exists—it is absurd on the face of it. Our exchange rules compare favorably with those of any exchange in the country, and such a rule is not to be thought of for an instant.

If the correspondent has a grievance against any dealer here who is a member of our exchange he can report the matter to O. C. Alexander, superintendent of the exchange, and I am sure it will be fully investigated, as the exchange is willing and anxious to do all in its power to promote fair dealing among its members, both in their relations to each other and their shippers.

The Central Elevator has been weighing cars for the last two years at \$3 per car. It seems unaccountable that many of the shippers still refuse to avail themselves of this protection, particularly as the dealers here are willing to stand half the expense, which would make it cost shippers only \$1.50 per car. It seems to me that a shipper who is so blind to his interest as to decline to pay the small additional expense is not entitled to much sympathy. He may have been dealing with irresponsible parties who did not want to weigh correctly; but with the possibility of absolute protection in weights and plenty of reliable dealers to do business with he should be careful about reflecting on the general rules of business here. Very truly, R. S. McCague, Pittsburg, Pa.

Pacific coast farmers found insurance on standing grain very valuable the past two years.

President Warren of the Chicago Board of Trade has appointed W. H. Eckhardt, H. B. Slaughter, M. Churchill, Geo. S. Bridge and Geo. A. Stibbens to represent its interests in the Civil Service Reform Association. If the Association will succeed in placing the state grain inspectors under strong civil service rules which are rigidly enforced it will perform a valuable service for the grain trade of the country.

Stephen D. Fessenden, acting statistician of the Department of Agriculture, in his report issued Oct. 10 gives the average condition of corn Oct. 1 as 79.6, compared with 84.3 a month earlier and 52.1 a year ago. The spring wheat yield is estimated at 14.4 bushels and the quality as 87.7 per cent. The average yield of oats is estimated at 34.5 bushels, compared with 25.1 bushels a year ago and 26.8 bushels, the average for 10 years. The average quality of the present crop is 86.7.

ASKED AND ANSWERED

HOW TO BLEACH OATS.

Grain Dealers Journal: On page 99 of the Journal issued July 25 is shown a cut of a European device for bleaching oats or other grain. Our crop of oats this year is more or less stained. We have seen some of the bleached stock, and it is certainly a great improvement to the oats as far as attractiveness is concerned. We also have quotations from Boston showing that where oats were treated in that manner it improved their selling value from 3 to 4 cents per bushel.

Can some reader of the Journal give measurements and more details of the device?

I understand the pure food laws of Illinois are against this kind of bleaching, but if the goods are not adulterated nor injured, but actually improved by the process, why should not a grain man take advantage of all the devices he can obtain to improve his goods, especially for export, which I understand the trade demands, if they pay the prices indicated by Boston reports?—R. K.

[For information write the American Grain Purifier Constructing Co. The Illinois law has been declared unconstitutional and void.]

ARE LAWS GOVERNING WEIGHING OF GRAIN LEGAL?

Grain Dealers Journal: The constitutionality of the act of 1883, providing for state weighmasters and entitled "An act to provide for the appointment of state weighmasters," has recently been brought to my attention by the appointment by Mr. Neville, chairman of the Railroad and Warehouse Commissioners, of certain weighmasters under the aforesaid act and instructions to the owners of warehouses and elevators in which these weighmasters were to exercise their duties not to allow any other man than the appointed weighmasters to officiate in these warehouses and elevators.

Of course, it must be obvious that this act of 1883 was an outcome and extension of the legislation in 1871, by which was passed "An act to regulate public warehouses and the warehousing and inspection of grain, and to give effect to article 13 of the constitution of this state." By article 13 of the new constitution of 1870 all elevators and storehouses where grain or other property is stored for compensation are declared to be public warehouses. This article also provides "that the General Assembly shall pass laws for the inspection of grain, for the protection of producers, shippers and receivers of grain and produce." In pursuance of such authority and direction under the constitution of 1870 the legislators of the next year passed the "Act to regulate public warehouses and the warehousing and inspection of grain" referred to supra, and also an act entitled "An act to establish a Board of Railroad and Warehouse Commissioners," and prescribing their powers and duties.

Obviously such legislation fixing the maximum rates to be charged for the storing of grain and providing for state inspection, etc., was bound to be litigated. It was unmistakably sweeping legislation and property to the extent of millions of dollars was involved. From time immemorial in England and the United States from the beginning, it had been customary for the government to regulate ferries, hackmen, common carriers,

bakers, millers, wharfingers, inn-keepers, etc., fixing the maximum of charge for services rendered, accommodations furnished and articles sold. The question was now raised, could the state legislature, under the fourteenth amendment to the United States constitution, which forbade the taking of property without due process of law, regulate and fix the prices to be charged for the storing of grain? Evidently, down to the time of the fourteenth amendment, it had never been questioned that state legislators, in some instances at least, could regulate the use and even the price of the use of private property, and still be guiltless of taking property without due process of law. Did the fourteenth amendment change this, and, if so, how?

Plainly, what would have been taking property without due process of law before the fourteenth amendment would have been such process thereafter, but would what was taking property with due process before become undue process thereafter? Obviously the fourteenth amendment made it impossible for the state of Illinois thereafter to deprive one of property without due process of law, but clearly she could not have done this before the fourteenth amendment, for by the constitution of 1848 this identical restraint was imposed upon her legislators. This amendment to the federal constitution in effect then only guaranteed the people of the state the continued preservation of a right which had theretofore never been denied them by the state. If, then, before and after the fourteenth amendment the legislature could legitimately regulate and control certain businesses to the extent even of fixing the maximum charge for services rendered, accommodations furnished and articles sold, as is instanced in the case of the ferries, common carriers, etc., the only question was, could the business of the management of grain elevators be added to this list of businesses in which the government might interfere with regulations and restrictions?

This vital question was finally answered in the famous case of *Munn v. Illinois*, 94 U. S. 115, and no better understanding of it can be gained than by quoting right here from Chief Justice Waite in that part of his opinion where he says: "It is difficult to see why if the common carrier, or the miller, or the ferryman, or the inn-keeper, or the wharfinger, or the baker, or the cartman, or the hackney-coachman pursues a public employment and exercises 'a sort of public office,' these plaintiffs in error do not. They stand, to use the language of their counsel, in the 'very gateway of commerce,' and take toll from all who pass."

Since this decision settled the constitutionality of the inspection of grain and the fixing of maximum charges for storage of grain by state officials, it was not surprising that the act of 1883, which provided for state weighing of grain, was not questioned as to its constitutionality, for it is difficult, as one eminent Chicago lawyer has said, "to make a distinction between a qualitative and quantitative analysis of grain." However, since this recent appointment of state weighmasters by Mr. Neville, with instructions to the owners of grain elevators not to allow other persons in their employ to officiate in like capacity, it has occurred to me whether or not such instruction was such that for its disobedience the owner of a grain elevator was subject to a penalty.

In a line of cases, of which *United States v. Eaton*, 144 U. S., 667, is the leading one, it has been held that a section

of a statute fixing a penalty for neglect to do a thing required by law, does not extend to the case of neglect to do a thing required by the regulations of a Board of Commissioners. Upon examination of these statutes upon which these cases were litigated, however, it will be found that in the sections providing that commissioners make regulations as they see fit, no penalty was provided for the disobedience of such regulations after made by the commissioners, and it was held that a subsequent section of the statute providing a penalty for neglect to do a thing required by law did not extend to the case of neglect to obey the regulations of the commissioners. Upon an examination of the act of 1883 in regard to weighmasters, it will be found that it is provided that the weighmasters are to have exclusive control of the weighing in the elevators and a penalty is fixed for refusing or preventing such weighmasters in performing their duties under the Act. Mr. Neville then, in instructing the owners of elevators not to allow any other persons than commissioned ones to weigh grain in their elevators, was no more than saying, "You shall not interfere or prevent these commissioned weighmasters from performing their duty." He is only making a rule for the violation of which the statute has expressly fixed a penalty, and it would seem that no constitutional objection can be taken to such a regulation by the commissioners.

There, however, remains one other point upon which it is not so clear to me that the act is free from objections, as it relates to and affects the Chicago Board of Trade. It seems that the Chicago Board of Trade was incorporated in 1859, and that by section 10 the board was granted the privilege of appointing persons "to examine, measure, weigh gauge or inspect grain and other articles of produce and traffic commonly dealt in by members of such corporations." There is no provision that such charter was given subject to be altered, repealed or amended. Here was a right granted a corporation for which they gave consideration. Indeed, did not this very privilege, granted them by the legislature of 1859, constitute one of the chief contract rights granted under this act of incorporation? Surely such an inducement held out by the state must be considered one of the chief factors in securing the charter. Now, subsequently a legislature comes along and entirely sweeps away this privilege and contract right, for which a valuable consideration was given, by giving the exclusive control of the weighing and inspection of grain into the hands of state officials. Why is not this legislation, as far as it affects the Chicago Board of Trade, an impairment of the obligation of contract? This is a question upon which I am not so clear, and at present am unable to see why this legislation as it affects the Chicago Board of Trade is not open to this constitutional objection. Very sincerely, Ira M. Allen, Home Insurance Building, Chicago.

The pain in the pocketbook is forcing grain men to be more intelligent in competition.

Beans and peas amounting to 156,447 bushels, valued at \$319,086, were exported during the first eight months of 1902, while 279,362 bushels, valued at \$519,731 were exported during the corresponding months of 1901, and 319,886 bushels, valued at \$571,044, during the corresponding months of 1900.

Meeting of Miami Valley Dealers.

The Miami Valley Grain Dealers Association met at Wapakoneta, O., in Hotel Stenberg, Oct. 23, and had one of the most enjoyable meetings of its existence. The association was organized July 16, 1902, and has over 65 members. This speaks well for so young an organization, and is due to the general good feeling, and efforts of its officers. Grant McMorran of St. Paris is president and H. M. Allen of Troy is secretary.

At 8:30 p. m. the members were seated in the dining room and the banquet (served in several courses) was in its glory.

MENU.

New Yorks Counts.
Bouillon en Tasse.

Celery. Salted Almonds. Olives.
Lobster a la Newberg. Potatoes Julienne.
Roast Young Turkey, Dressing and Cranberries.
Asparagus Vinaigrette.
Glazed Sweet Potatoes.
Tutti Frutti Ice Cream. Assorted Cake.
Fruit.
Edam Cheese. Wafers.
Cafe Noir.
Wine and Cigars.

Joshua Grubbs, J. R. Shuman, Covington; Leroy Blose, Urbana; Jos. F. Copcock, W. O. Patty, Fletcher; J. N. Thatcher, Chas. Tirrel, Frank Felger, De Graff; J. W. Sherer, J. C. Wones, L. N. Baker, Maplewood; Geo. L. Runkle, J. Allen, St. Johns; J. Detrick, Tippecanoe City; E. A. Brecount, Joe Walcott, Conover; Geo. Leggett, Westville; J. E. Wells, W. H. Prusing, Quincy; T. B. Marshall, Kirkwood; M. H. Long, A. E. Clutter, Lima; W. J. Jenkins, F. D. Burbank, Grant McMorran, John Duncan, St. Paris; C. N. Adlard, M. W. Miller, Piqua; Jos. Myers, Frank Folk, C. F. Tennerman, J. W. Woodward, St. Marys; Zenas Felger, Lewiston; C. A. Bissell, Antwerp.

Director-at-Large.

No doubt some persons temporarily gifted with double vision have seen two or the new Directors-at-Large of the Grain Dealers National Association, but few, if any, have ever seen five of them at the same time.

Herewith we present him in Quintuple. Anyone who is a good judge of human

Wiedmer, F. W. Selee, A. P. Richardson, and J. E. Hall of St. Louis; J. S. Davant, commissioner of the Memphis (Tenn.) Freight Bureau; W. P. Trickett, commissioner of the Kansas City Transportation Bureau; H. C. Bradford and C. N. Wisner of Memphis, F. C. Vincent of Chicago, E. C. Buchanan and W. G. Cannon of Memphis, H. F. Halliday and J. B. Magee of Cairo, Ill.; R. Pendleton of St. Louis and Chicago, M. L. Dyer and C. M. Bullitt of Henderson, Ky.

Hot Corn.

HOT CORN seems likely to make much trouble for shippers from many sections of the country this season, although Southern Illinois, Southern Indiana and Missouri report much corn will soon be dry enough for shipment. Some sections of Iowa and Ohio have had so very much more rain than other sections, that the corn is greener than elsewhere. However, unless great care is exercised in sorting corn before it goes to the sheller and in drying before loading, heavy losses are sure to accrue. The crop has not yet begun to move, yet rail carriers are so



Director-at-Large of the National Association—J. W. Snyder, Baltimore, Md.

After the members were well filled, speeches on the following subjects were given.

Relations and Organization, E. E. Nutt of Urbana.

Loaning of Bags, E. A. Grubbs of Greenville.

Our Insurance, J. W. McCord, secretary of the Ohio Grain Dealers Association, Columbus.

Claim Notice with Each Shipment, Percy R. Hynson, Columbus.

Remarks on Relations of Darke County Association, Ed. McCue, Pittsburgh.

Corn, John Duncan, St. Paris. Followed by remarks as to time of buying corn.

Meeting adjourned at 2 a. m. Friday.

Members present: A. Stinger, Jacob Hauss, Wm. Bitler, G. A. Buck, Wapakoneta; J. W. Simmons, Pemberton; Percy R. Hynson, J. W. McCord, Chas. O. Peters, representing the Ohio Grain Dealers Mutual Fire Insurance Association; E. A. Grubbs, Greenville; J. W. Shultz, J. C. Paul, Botkins; W. L. Allton, Swanders; G. E. Allinger, Jackson Center; A. M. Duncan, Uniopolis; Jos. Timmons, Kenton; Ogden Edwards, H. M. Allen, A. L. Beadle, Troy; A. G. Borgher, Santa Fe; A. H. May, New Bremen; W. N. Dingledine, Buckland; Emory C. Nutt, E. J. Griffis, Sidney; Ed. McCue, Pittsburgh; E. F. Reichelderfer, Cridersville;

nature can readily discern that he is an all around grain man.

Jno. W. Snyder is of the firm of Hammand & Snyder, Baltimore, Md. He has long been identified with the grain trade and taken an interest in association work. It is not to be supposed that he will attempt to do five men's work in the National Association this year, although some may now claim such should be required of all members who appear in quincunx.

Lower Gateways Association.

At a meeting at St. Louis, Mo., Oct. 14, the Grain Dealers Association of the Lower Gateways was formed to look after the interests of the grain shippers handling grain thru river points south of Cincinnati and Louisville.

H. E. Halliday of Cairo was elected chairman, and J. B. Magee of Cairo, secretary.

An executive committee composed of H. E. Halliday, Cairo; R. Pendleton, St. Louis; John Wiedmer, St. Louis; M. L. Dyer, Henderson; and C. N. Wisner, Memphis; was appointed to see that rates are not manipulated unfairly.

A second meeting is soon to be held to plan action in the courts or with the railroads to equalize rates in the Southeast.

Among those present were: M. Kennedy, E. L. Bartlett, W. B. Harrison, John

badly handicapped with freight in excess of their capacity to move, that cars are being delayed one to two months in many sections of the country. Truly this is not very cheerful news to the shipper of new corn, but the fact remains that the carriers are just as anxious as ever to get shipments. Naturally they desire to get control of the freight. Many country buyers are disposed to postpone receipt of new corn as long as possible in hope of averting disastrous losses. Money in the bank does not often deteriorate, but when tied up in hot corn which has been locked up in a box car for six to eight weeks, is not likely to realize many cents on the dollars originally invested.

A maltster at Chicago recently bot a car of white oats to make malt.

The fall meeting of the Ohio Grain Dealers Association at Columbus was the best meeting ever held, there being about 125 shippers present from all parts of the state. Many matters of interest were discussed, and every one who attended was benefited. Nearly all were in favor of putting off the buying of new corn as long as possible owing to its condition. The meeting only lasted one day, but those who took the trouble and time to go there were fully repaid.—Writer certainly feels that he was. These grain dealers meetings are a grand thing.—J. F. Zahm & Co.

SEEDS.

S. A. Muff, New Carlisle, O., Oct. 10: Clover seed a failure.

Spangler Bros., Defiance, O., Oct. 9: Clover seed almost total failure.

Baldwin & Elliott, West Liberty, O., Oct. 11: Clover seed sprouting badly.

J. F. Plice, Nankin, O., Oct. 11: Clover seed very light crop and poor quality.

J. E. Wells & Co., Quincy, O., Oct. 11: Clover seed reduced in yield and quality by rain.

The Springfield Seed Co. of Springfield, Mo., has increased its capital stock from \$12,000 to \$20,000.

Tenney & Morgan, Newark, O., Oct. 10: Clover seed damaged by rains on the plant already cut for seed.

O. W. Leoffler, Bad Axe, Mich., Oct. 18: Clover seed promises about one-half of a crop and that a poor article.

Wm. Salen & Co., West Salem, O., Oct. 10: Clover seed will not be over one-fourth of a crop owing to excessive rains.

Coppess & Dohme, Stelvideo, O., Oct. 11: Lots of clover will not be hulled as so much rain has caused it to sprout badly.

C. D. Marshall, who conducts a wholesale and retail field seed business at Brunswick, Mo., has a large warehouse fully equipped for this line of trade.

The Steele-Briggs Seed Co., Limited, of Toronto, Ont., has purchased the seed business at Winnipeg, Man., of J. M. Perkins, who retains an interest in the business and will devote his time to the work.

Flax and timothy seed imported into the Philippine Islands during the seven months ending Jan. 31, amounted to 19,869 pounds, as reported by the War Department; compared with 662 pounds imported during the corresponding months of 1900-1.

Chicago shipped during the week ending Oct. 18 565,625 pounds of timothy seed, 73,700 pounds of clover seed, 278,500 pounds of other grass seed and 46,726 bushels of flaxseed; compared with 632,834 pounds of timothy seed, 37,338 pounds of clover seed, 172,273 pounds of other grass seed and 6,606 bushels of flaxseed shipped during the corresponding week of last year.

Chicago received during the week ending Oct. 18 1,465,300 pounds of timothy seed, 47,110 pounds of clover seed, 175,150 pounds of other grass seed and 76,400 bushels of flaxseed; compared with 1,249,685 pounds of timothy seed, 171,490 pounds of clover seed, 284,600 pounds of other grass seed and 123,880 bushels of flaxseed received during the corresponding week of last year.

Rain has damaged considerable clover. Shippers should look out for this class of seed when it comes to market. It will have a very bad color, and will probably sell at quite a large discount under prime. We would advise shippers to keep this class of seed separate from seed that is of bright color. Some samples of seed we received lately are very poor. The seed was out in the rain and badly damaged.—J. F. Zahm & Co.

Receipts of clover seed at Toledo, O., for the week ending Oct. 18, were 2,245 bags, making a total for the season of 24,656 bags; compared with 7,460 bags received during the corresponding week of last year and 52,876 bags received last season. Shipments for the week were 739 bags, making a total of 5,309 bags for

the season; compared with 3,455 bags shipped during the corresponding week of last year and 18,336 bags shipped last season.

A number of varieties of clover and grass seed were tested for five consecutive years and the germinative energy during the four years was as follows: Red clover lost in this time 17.5 per cent; alsike, 79.4 per cent; white clover, 32.4 per cent; alfalfa 23 per cent; tall oat grass 51 per cent; perennial rye, 50 per

Confirmation of Sale.

A form of confirmation of contract in which the main points are given special prominence is presented herewith.

When a sale has been effected, which may be by wire, by 'phone, by letter or by traveling man, the track bidder mails the shipper the two duplicate blanks, one to be retained by the shipper, the other to be returned to the buyer.

CONTRACT OF SALE

TO

THE CLEVELAND GRAIN COMPANY,

INCORPORATED.

CLEVELAND, OHIO.

DEAR SIRS: We confirm sale to you to-day by

No. Bushels,

Kind of Grain,

Price,

Time Shipment,

Cleveland, Ohio, Weights and Grades guaranteed.

☒ Above Grain to be Billed to The Cleveland Grain Co., Cleveland, Ohio, via Big 4 R. R.

Extension of time for shipments, or cancellation of contracts, ONLY AT OPTION of and per agreement with BUYER.

When grain is not shipped within specified time, contracts are considered open until shipped or we are notified that you have closed same.

Off grades to be applied on purchases at market difference if salable grain.

☒ PLEASE SIGN AND RETURN.

Yours truly,

cent; meadow fescue, 40.5 per cent; timothy, 9.4 per cent; orchard grass, 7.3 per cent; red top, 44.8 per cent; sheep fescue, 70 per cent.

A Canadian dealer writes C. A. King & Co.: Our advices regarding the outturn of this season's crop of clover seed in both Canada and United States seem to be going from bad to worse, and the crop is daily shrinking into smaller proportions. Advices from Britain and Europe also indicate a crop far below an average, and very much less than anticipated up to the beginning of this month. With such a serious shortage in the clover seed crops of both America and Europe, and very light stocks carried over, prime clover seems exceedingly good property up to \$7.50.

Our exports of malt during the seven months ending July 31, were 255,218 bushels; compared with 234,146 bushels exported during the corresponding period of 1901.

Broomcorn valued at \$86,376 was exported during the seven months ending July 31; compared with \$125,289 worth exported during the corresponding months of 1901.

The vital points are given on the form returned to the buyer. On the form retained by the shipper are given minor points also, as well as instructions for billing, drafts and invoice. The price is filled in after the word price; for example: "37 cents f. o. b. Big 4 track, Smiths-ville, Ill." the seller's station.

To easily distinguish the blanks one is printed on white and the other on yellow paper.

In case of failure to ship within specified time the buyer may consult his own pleasure as to extension of time or cancellation of contract, to which there can be little objection if the seller is promptly advised by wire of the buyer's action. Otherwise the want of a definite statement on the face of the contract fixing the number of days after expiration that the sale was to be bot in would leave the seller unable to estimate the penalty he would be required to pay.

In the form given the Cleveland Grain Co. believes it has one that covers the whole ground and is binding. Criticism is invited.

Contract Grades of Chief Inspectors Association.

Secretary J. D. Shanahan of the Chief Grain Inspectors National Association informs us that the rules governing the contract grades of grain as adopted by the Inspectors Association at Peoria are as follows:

NO. 1 RED WINTER wheat shall be pure red winter wheat either or both light and dark colors of the short berry varieties. Sound, plump, dry, sweet and clean, and weigh not less than 60 pounds to the measured bushel.

NO. 2 RED WINTER wheat shall be

sound, dry and reasonably clean and weigh not less than 58 pounds to the measured bushel.

NO. 1 HARD WINTER wheat shall be pure hard winter wheat. Sound, dry, plump, sweet and clean and weigh not less than 61 pounds to the measured bushel.

NO. 2 HARD WINTER wheat shall be hard winter wheat. Dry, sound, sweet and reasonably clean and weigh not less than 59 pounds to the measured bushel.

[The chairman of the committee on rules for grading spring wheat failed to report and for this reason and the limited time the question of spring wheat rules was not taken up.]

bright and clean and weigh not less than 32 pounds to the measured bushel.

NO. 2 WHITE oats shall be 95 per cent white oats. Dry, sweet and reasonably clean and weigh not less than 29 pounds to the measured bushel.

NO. 1 MIXED oats shall be mixed white and black or brown oats. Dry, sound, plump, sweet and clean and weigh not less than 32 pounds to the measured bushel.

NO. 1 CLIPPED white oats shall be pure clipped white oats. Dry, sound, plump, sweet, bright and clean and weigh not less than 35 pounds to the measured bushel.

NO. 2 CLIPPED white oats shall be 95 per cent clipped white oats. Dry, sweet and reasonably clean and weigh not less than 32 pounds to the measured bushel.

NO. 1 CLIPPED MIXED oats shall be clipped mixed white and black or brown oats. Dry, sound, plump, sweet and clean and weigh not less than 35 pounds to the measured bushel.

NO. 2 CLIPPED MIXED oats shall be clipped mixed white and black or brown oats. Dry, sweet and reasonably clean and weigh not less than 32 pounds to the measured bushel.

NO. 1 RUSTPROOF oats shall be pure rustproof or Texas red oats. Dry, sound, plump, sweet, bright and clean and weigh not less than 32 pounds to the measuring bushel.

NO. 2 RUSTPROOF oats shall be 95 per cent rustproof or Texas red oats. Dry, sweet and reasonably clean and weigh not less than 29 pounds to the measured bushel.

NO. 1 CLIPPED RUST PROOF oats shall be pure clipped rust proof or Texas red oats. Dry, sound, plump, sweet, bright and clean and weigh not less than 35 pounds to the measured bushel.

NO. 2 CLIPPED RUST PROOF oats shall be 95 per cent clipped rust proof or Texas red oats. Dry, sweet and reasonably clean, and weigh not less than 32 pounds to the measured bushel.

NO. 1 RYE shall be dry, sound, plump, sweet and clean and weigh not less than 57 pounds to the measured bushel.

NO. 2 RYE shall be dry, sound, sweet, reasonably clean and reasonably free from other grain and weigh not less than 55 pounds to the measured bushel.

NO. 1 WINTER barley shall be dry, sound, sweet, plump, bright, clean and free from other grain.

NO. 2 WINTER BARLEY shall be dry, sound, sweet and reasonably clean, but not plump or bright enough for No. 1. May be slightly broken but free from other grain and weigh not less than 48 pounds to the measured bushel.

SPRING BARLEY shall be graded under the same rules as winter barley except that it shall be designated as spring barley.

The association has also formulated rules to govern the lower grades of the above grains, which in due time will be published and the different exchanges and warehouse commissions who govern the inspection of grain in the United States will be asked to adopt them in the interest of uniform phraseology in the description of grades.

Secretary Geo. C. Dunaway of Utica, Ill., informs us that the Illinois Valley Grain Dealers Association will meet in regular session in the Columbia Hotel, Streator, on Wednesday evening, Oct. 29th, 1902. Business of importance.

C. C. WATKINS, President.

J. D. WATKINS, Treasurer

THE CLEVELAND GRAIN COMPANY,

(INCORPORATED.)

CLEVELAND, OHIO.

Indianapolis Office
ROOM 25 BOARD OF TRADE.

Indianapolis, Ind.

DEAR SIR: We confirm purchase from you to-day by

No. Bushels.

Kind of Grain.

Price.

Time Shipment.

Terms Cleveland, Ohio.

Billing Instructions, Bill to the Cleveland Grain Co., Cleveland, O., via Big 4 R.R.

Cleveland, Ohio, Weights and Grades.

Make Drafts on the Cleveland Grain Co., Cleveland, Ohio.

TO INSURE PAYMENT OF DRAFTS, PLEASE LEAVE AMPLE MARGIN
TO COVER WEIGHT AND GRADE CONTINGENCIES.

Always send invoice showing on what contract grain applies, and give weights or estimate of grain in ears, or on bill of lading, when you make draft.

Extension of time for shipment, or cancellation of contracts ONLY AT OPTION of and per agreement with BUYER.

When grain is not shipped within specified time, we consider contracts open until shipped or we notify you that we have closed same.

Off grades to be applied on purchases at market differences if salable grain.

Please bill all grain ORDER and notify the Cleveland Grain Company, or if you prefer, bill to YOUR OWN ORDER, notify the Cleveland Grain Company. If you bill to your own order, BE SURE TO ENDORSE BILLS OF LADING.

When you accept our bids please name price, and amount in bushels.

If above is not correct please notify us at once.

Yours very truly,

THE CLEVELAND GRAIN CO.

Per

Form for Confirmation of Contract.

red winter wheat. Sound, dry, sweet and reasonably clean, contain not more than 5 per cent of white winter wheat and weigh not less than 58 pounds to the measured bushel.

NO. 1 WHITE WINTER wheat shall be pure white winter wheat. Sound, plump, dry, sweet and clean and weigh not less than 60 pounds to the measured bushel.

NO. 2 WHITE WINTER wheat shall be white winter wheat. Sound, dry and reasonably clean, contain not more than 5 per cent of red winter wheat and weigh not less than 58 pounds to the measured bushel.

NO. 1 MIXED WINTER wheat shall be pure mixed red and white winter wheat. Sound, dry, plump, sweet and clean and weigh not less than 60 pounds to the measured bushel.

NO. 2 MIXED WINTER wheat shall be mixed red and white winter wheat.

NO. 1 YELLOW CORN shall be pure yellow corn. Sound, plump, dry, sweet and clean.

NO. 2 YELLOW corn shall be 95 per cent yellow. Dry, sweet and reasonably clean, but not sufficiently sound and plump for No. 1 yellow.

NO. 1 MIXED corn shall be mixed corn. Sound, plump, dry, sweet and clean.

NO. 2 MIXED corn shall be mixed corn. Dry, sweet and reasonably clean, but not sufficiently sound or plump for No. 1 mixed.

NO. 1 WHITE corn shall be pure white corn. Sound, dry, plump, sweet and clean.

NO. 2 WHITE corn shall be 98 per cent white corn. Dry, sweet and reasonably clean, but not sufficiently sound and plump for No. 1 white.

NO. 1 WHITE oats shall be pure white oats. Dry, sound, plump, sweet,

Meeting of Grain Dealers' Union.

The fall meeting of the Grain Dealers Union of Southwestern Iowa and Northwestern Missouri was called to order by President Hunter at 2:30 p. m. Oct. 14 in the Masonic Hall, Creston.

President Hunter said: It is not necessary to say that we are glad to see you. We have said that so often that it has come to be an old story. This meeting was called at the suggestion of two or three dealers who have had a little trouble in different parts of this territory. While it has not materialized, it is liable to, and we thought it best to settle it before the new crop began to move, and you could all come together and discuss the situation. The handling of this present crop is an enigma. We do not know where to begin, when to begin nor what we are going to pay. We can't form any opinion from the market. Consequently, we ought to discuss that matter thoroughly to-day and come to some conclusion as to what we are going to do under existing circumstances, or circumstances that may come up in the future. I don't know where to begin, but I believe in order to start this matter it would be a good idea for a number of those present to give us the condition of their crops around their immediate vicinity—about what the yield is and the condition of the corn. We will begin with Mr. Allbright. Also while making your statement, state whether or not harmony exists at your station between yourselves and your competitor and if you know of any trouble at your nearby stations.

Mr. Allbright, Lewis: I can't tell very much about the corn crop yet and I think no one else can. I think it will be pretty soft and hard to handle, probably it will run from 40 to 50 bushels per acre.

Secy. Stibbens: What per cent will be soft?

Mr. Allbright: I have an idea there will be about one-fourth soft. It is going to be pretty poor except on the hills. In the sloughs it will be pretty soft. As far as harmony is concerned I guess we are all right. I haven't heard of any trouble.

J. D. Young, Anita: I am from the northeastern part of Cass County. The acreage around us this year is increased from what it usually is from 8 to 10 per cent. The corn is not quite up to the average on account of frost in the early part of September. Twenty-five per cent of the acreage will be damaged from 35 to 50 per cent. The condition of the corn at the present time is hard to determine on account of having had so much rainy weather. If it continues of course a greater per cent of the corn will be bad, but if it stops and the present weather continues it won't be quite so bad. Prices we have been paying have been 25 cents per bushel; some have been paying as high as 30 cents. As far as conditions relating to competitors, we have no trouble whatever. The average yield will be about 45 to 50 bushels to the acre. There will be more feeding than usual.

P. Danner, Blanchard: We have raised a fairly good crop, in fact a very good crop. As reported by those who have preceded me, there will be considerable soft corn. If this last rain had not come, the corn probably would have commenced to move this week. In regard to harmony, I guess we haven't as much as we ought to have but I am in hopes

matter will be so settled we will have no difficulty in that direction. The average is from 40 to 60 bushels to the acre, probably an average of 50.

J. R. Giles, Lenox: I can give the condition of the crop pretty well and I will leave it to Mr. Hewitt to give the harmony part of it. Perhaps 60 to 75 per cent is No. 3 corn this fall and is of a quality that will eventually make No. 2. The other 25 to 30 per cent is quite poor. The poor grade of corn is very light. In regard to harmony: we have plenty of it but it is of a very poor kind.

The local conditions at Lenox were discussed at some length.

O. A. Talbott, Osceola: All I can say is we have a fairly good crop. As far as harmony goes we do not have very much trouble. The average yield will be about 40 bushels to the acre.

Joe Wray, Creston: I do not know I can add anything to what has been said. I think we have a fair crop. The amount of soft corn is probably one-third. As far as harmony goes we have plenty of it. The yield will be 40 to 50 bushels.

M. Hennessy, Orient: I think many have estimated the yield too high. I will say that we have considerable soft corn in our section of the country. I guess the conditions are about the same as in other parts of the state. As far as harmony is concerned between myself and the Orient Grain Company, I guess it is all right. I admit I do some things myself sometimes I am a little ashamed of, and I guess they are about the same way.

Mr. Hennessy's remark was greeted with laughter.

W. Daugherty, Hawthorne: I guess the corn is about the same all over. A little poorer here and a little better there. We have considerable very soft corn. From the best information, I think our corn will average between 45 and 50 bushels. Some of the farmers are commencing to move the corn and some will commence next week. The corn averages 45 to 50 bushels.

M. F. Hackett, Fairfax, Mo.: We have a very good corn territory, and I think, as near as I can put it, in my territory our corn will make about 90 per cent good sound corn. I think I am safe in saying that we will have fully 90 per cent of good marketable corn, better than we have had for two or three years. The yield from the best information I have now will be close to 50 bushels. The corn has very nice large ears, and we have no worms in the corn this year at all. We have been troubled for the last two years with worms in the points of the ears. We will commence to gather corn there right away. Some have commenced already but there will not be a great part of our corn crop moved before the first of next month. As to harmony, we have no trouble. The feeders are my competitors. Corn sold to feeders brings 30 cents.

After considerable discussion of local matters, the following applications for membership were read by Secy. Stibbens, and their acceptance moved and carried:

J. A. Archer, New Market, Ia.; A. H. Bailey, Forest City, Mo.; Blanchard Mill & Elevator Co., Blanchard, Ia.; F. E. Cowden, Riverton, Ia.; Cummings Bros., Nodaway, Ia.; A. D. Franshaw, Ridgeway, Mo.; Hulshizer & Co., Hamburg, Ia.; P. Kilmartin, Malvern, Ia.; Mudgett & Henry, Blythedale, Mo.; Nebraska-Iowa Grain Co., Farragut, Ia.; T. S. Spencer, Afton, Ia.; Stearns & Co., Essex, Ia.

The meeting adjourned at 5:30.

CONVENTION NOTES.

The attendance was about forty-five. The weather was delightfully clear and cool.

The Grain Dealers Journal was represented.

One man from Missouri: M. F. Hackett of Fairfax.

The Nebraska-Iowa Grain Co., Omaha, was represented by K. C. Moorehouse.

The Governing Committee was called upon to adjust one or two local matters.

The Wabash Railroad was represented by Geo. M. Entrikin, division freight agent.

Charlie Boynton was there as usual with plenty of cigars and smiles. He also supplied the markets.

The Weare Commission Co. was represented by Mr. Hunter of Des Moines and S. R. Parke, its new Creston manager.

The business was disposed of at the afternoon session and it was not necessary to hold a night session, as some had thought probable.

Jos. Norton represented the P. P. Williams Grain Co. of St. Louis. C. V. Fischer & Co., St. Louis, were represented by C. V. Fischer.

M. F. Hackett wore a broader smile than usual. In fact showed he had been exercising his most pleasant expression lately. He is candidate on the Republican ticket for County Clerk of Atchison County, Missouri.

The Iowa dealers present were: W. W. Allbright, Lewis; J. Auracher, Shenandoah; W. F. Buffington, Glenwood; F. M. Campbell, Randolph; G. H. Currier, Prescott; C. F. Davis, Pacific Junction; O. G. Danner, Blanchard; W. D. Daugherty, Hawthorne; D. N. Dunlap, Fontanelle; W. H. Eaton, Emerson; H. B. Farquar, Orient; D. Gault, Cromwell; Jas. Gault, Creston; J. L. Gwynn, Imogene; John Gilmore, Imogene; John R. Giles, Lenox; M. Hennessy, Orient; W. M. Hewitt, Lenox; J. L. Hoak, Sharpsburg; J. H. Hulbert, Fontanelle; O. T. Hulburd, Osceola; D. Hunter, Hamburg; E. C. Kayton, Strahan; P. Kilmartin, Malvern; T. A. Kyle, Shenandoah; T. J. McCormick, Stanton; Wm. MacMahill, Shenandoah; Mr. Oline, Page Center; B. C. Ragan, Coin; C. C. Ragan, Northboro; H. Ragan, Elliott; W. G. Sherman, Riverton; T. S. Spencer, Afton; O. A. Talbott, Osceola; H. D. Turner, Elliott; N. N. Turner, Cumberland; Geo. Van Buskirk, Shenandoah; E. H. Van Schoiack, Elliott; G. A. Willett, Osceola; Geo. E. Wilson, Hepburn; Joe Wray, Creston; W. Wyant, Malvern; J. D. Young, Anita.

Meetings of the dealers in the neighborhood of Goshen and Valparaiso Oct. 23 and 24 respectively. Much interest was shown in the local work.

A meeting of the Central Indiana Division of the State Association will be held at Logansport, Thursday, Oct. 30, 10 a. m. A large attendance is desired.

The Board of Managers of the Indiana Grain Dealers Association has voted to expel Wm. Nading of Shelbyville for refusing to arbitrate a difference with the Cleveland Grain Co.

People in the trade are old enough to remember the time when corn was not allowed to grade No. 2 in Chicago before the May following its maturity. The change in the rules was made to allow of corn coming here from the southwest, and no one then expected corn from Iowa to grade in December under the new rule.—Irwin, Green & Co.

THE SUPPLY TRADE

SUITS AND DECISIONS

Where articles are well known to the public, sales are more easily made and satisfaction more easily assured to the purchaser.—*Progressive Advertiser*.

The Cleveland Grain Drying Co., incorporated under the laws of New Jersey at Cleveland, O., has increased its capital stock from \$20,000 to \$25,000.

The Marseilles Mfg. Co., manufacturers of grain elevator and mill supplies and elevating and conveying machinery, is building an addition to its plant at Marseilles, Ill.

The Union Scale & Mfg. Co., San Francisco, Cal., has recently furnished five automatic grain scales for the addition to the plant of the Postum Cereal Co., Battle Creek, Mich.

Philip Moser of Sidney, O., mechanical engineer and millwright, now is kept busy supplying the trade with his New Era Elevator, a device that saves the elevator man many climbs between working floor and cupola.

A. J. Clark of Minneapolis, Minn., has recently completed a 25,000-bushel hopped bin elevator for the Nebraska Grain & Live Stock Co. at Bloomfield, Neb. It is equipped with rope drives and a 10-h.p. gasoline engine.

It is most natural that a man should avoid publicity in all things relating to his private life. But the merchant who seeks the same privacy in his business affairs is sure to go to the wall. Publicity is the genius of success in business.

Tweedale & Harvey, R. D. Atchison, general manager, Chicago, are installing their Perfection Grain Drier in a number of country grain elevators. A Perfection Drier has recently been installed in the plant of Caldwell & Barr, Earl Park, Ind.

The Jeffrey Mfg. Co. of Columbus, O., has its new catalog No. 72 ready for mailing. This book illustrates and describes the Jeffrey Co.'s complete line of elevating, conveying and power transmitting machinery, and should be in the hands of every operator of grain elevators. Write for a copy.

If you toot your little tooter and then lay aside your horn, there's not a soul in ten short days will know that you were born. The man who gathers pumpkins is the man who ploughs all day, and the man who keeps it humping is the one that makes it pay. The man who advertises with a short, a sudden jerk, is the man who blames the editor because it didn't work.

The best paint for exposed iron and wood is said to be graphite ground in pure linseed oil. How to put it on is fully explained in a handsome booklet just issued by the Wisconsin Graphite Co. of Pittsburg, Pa., and entitled "A Life Preserver." The booklet gives views of the company's mines and works at Junction City, Wis., the pulverizing works at Stevens Point, Wis., and the office and works at Pittsburg, Pa. The company will gladly mail a copy to any grain dealer who will send his address to the Pittsburg office.

The Nile flood this year is the lowest in the last twenty-five years, and although the scarcity of water will in part be compensated by the new works executed by the British Government, the crops in Upper Egypt are likely to suffer. Such water as is available will probably be chiefly applied to the cotton crop, leaving beans, lentils, maize, etc., to bear the chief effect of the scarcity.

Gill & Fisher, grain dealers of Baltimore, Md., have been restrained by the court on petition of Queen Anne's Railroad from bringing suit on bills of lading for wheat valued at \$2,000.

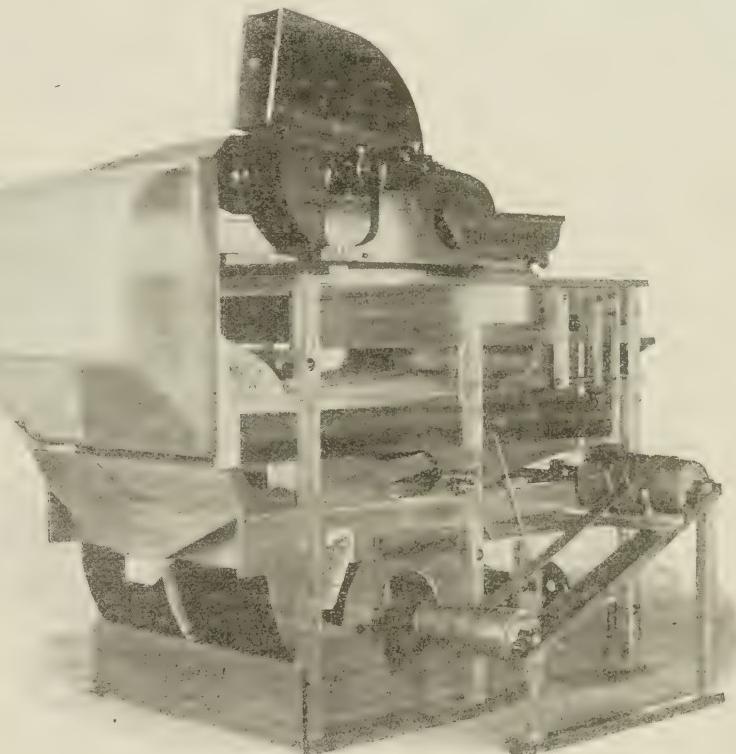
In a bucket-shop case the Iowa Supreme Court recently decided that a customer can recover money paid in as margins when he understands that the money was to be invested in the actual purchase of grain and not to be merely wagered on the fluctuations in quotations.

The Smith-Gambrill Co. of Baltimore, Md., has brot suit at Toledo, O., against

fayette terms. Still the Born Co. did not ship; and the Union Grain & Hay Co. had to buy in the corn at a loss of \$400, which the Born Co. refused to pay. When charged with unmercantile conduct the Born Co. suggested arbitration, which was granted. The arbitration committee decided against the Born Co. and an appeal was taken, only to be met by an adverse decision.

Improved Bean Cleaner.

The bad conditions under which the season's crop of beans has been harvested are providing a severe test for the efficiency of the bean cleaning machines in



The Clipper Improved Dustless Bean Cleaner.

the Union Railway Elevator Co., F. O. Paddock, James Hodge, Arthur L. Mills, to recover \$1,352, the value of two lots of oats destroyed thru the alleged carelessness of defendants in the burning of the Union Elevator in June, 1898.

Chas. Mann, dealer in grain and coal at Mayville, Wis., has brot suit against the Chicago, Milwaukee & St. Paul Railroad before the state railroad commissioner, alleging that the rate per ton on coal shipped from Milwaukee to Mayville is \$1, while between Milwaukee and Oshkosh, a much greater distance, the rate is only 75 cents.

It is expected that Samuel Born of the Samuel Born Co., Lafayette, Ind., will lose his membership in the Cincinnati Chamber of Commerce on account of his refusal to abide by the decision of the appeals committee of the chamber in the case in which his company was defendant and the Union Grain & Hay Co. of Cincinnati the complainant. Thru J. N. Wooliscroft the Samuel Born Co. sold 20,000 bushels of corn, No. 3 or better, to the Union Grain & Hay Co. for a specified delivery. The grain failed to arrive. Inquiry as to failure to ship brot out a statement from the Born Co. that the sale was on Lafayette terms, and the buyers then offered to take the corn on La-

use. The handler of beans whose machine is doing satisfactory work on such poor stock must consider himself fortunate.

A machine which is especially adapted to cleaning stock containing soft beans or beans that have become wet and partly decayed is shown in the engraving presented herewith. The Clipper Cleaner embodies the latest improvements. It has the roll and brush attachments, is dustless, and also makes an air separation on the stock as it passes from the hopper to the screens, removing all pods, dust and light refuse, thus relieving the screens of this matter. The screen work is made more effective and the capacity of the machine is increased. No man who likes to work amid clean surroundings can fail to appreciate the dustless attachment. Other improvements that need only be seen to be valued make this machine one of the very best of its kind.

Additional information and a statement of the dimensions and capacity of the different sizes will be given on application to the manufacturers, A. T. Ferrell & Co., Saginaw, Mich.

The German reichstag on Oct. 21 voted to make the minimum duty on rye \$1.37, and on wheat, \$1.50.

Fall Business Meeting of Ohio Grain Dealers Association.

The fall meeting of the Ohio Grain Dealers Association was called to order by President H. S. Grimes of Portsmouth in the Ladies' Ordinary of the Chittenden Hotel, Columbus, at 10:30 a.m., Oct. 14. He said:

It has been the practice of the association to hold two meetings a year, one for the election of officers and one for the discussion of trade topics.

About 40 of our members attended the annual meeting of the National Association in Peoria, which was a grand success.

Since our spring meeting our association has gained rapidly in membership until now it is stronger than ever. The association is doing the regular dealers much good and indirectly the farmers, for we have no more wild cat, irresponsible dealers who impose upon the farmers. The regular dealers always pay in full for what they buy from the farmers.

As an honor to Ohio your President was elected first Vice President. I am sorry we did not have a larger representation. All the members of this association are members of the National Association, and you are privileged to write to the Secretary of the National Association direct. You can have cases arbitrated thru that organization; and take up such matters with our State Secretary.

We will now hear from the Secretary.

Secretary J. W. McCord, Columbus: I will not detain you by reading the minutes of the last meeting, but I will tell you of the growth of the association. We now have 105 members at large, 58 affiliated from the Miami Valley Association, 48 from the Western, 39 from the Northwestern Ohio Grain Dealers and Millers Association, 31 from the Marion Association (Middle Ohio); a total of 281, with about 40 more yet to be reported to me by the Secretary of the Middle Ohio, who had not time to close his affiliation report in time for this meeting.

President Grimes: Instead of having a set program with papers we have selected subjects for discussion. I will depend upon you to discuss the subjects unsolicited, and I trust it will not be necessary to call upon anyone. We will first consider the new corn crop—"When should dealers begin to receive new corn; proper margin of profit to protect shrinkage, condition, grade and prospective market decline, to the level of values based on the deferred futures. Delay in securing cars."

R. F. Chenoweth, London: With favorable weather we will be able to move new corn by the last week of November. We need freezing weather. I would like it if we could get a margin of 10 cents a bushel. (Applause and laughter.) Corn is very soft and we take big chances. We might get along with 3 to 5 cents. I have been out in the fields and found soft corn still growing, milky and hanging down.

R. G. Calvert, Selma: Corn in the shock is soft and moldy. Corn must be sorted carefully. We ought to have a margin of 5 cents to start with. I do not think 70 lbs of ear corn will shell out 56 pounds.

F. E. Langdon, Wilmington: Corn is still growing. It will be a long time before it can be brot to market—probably first week in November. It will not grade before last of February or first of

March. Corn shipped before the middle of December is not likely to go cool and sweet. We will need all the margin we can get.

L. W. Dewey, Blanchester: I have seen some corn still standing, still growing, and the fodder is green, so doubt if it will be in marketable condition for two or three months. Dry corn in the shock may be shipped sooner. I see too many dealers doing a thing because someone else does it. Let each dealer forget his neighbor. Do not buy any grain unless you have a profit in it. Margins should be what we can get. We should figure on twice as much margin as usual.

G. P. Teegarden, Ashville: I doubt if any corn can be moved before November 10 to 15th. I have seen some very dry corn. I think to be safe we must buy on a larger margin than usual.

H. S. Heffner, Scircleville: We will have a good crop of corn and will start to milling it next week. We dry it first. We do not often talk of profits at Scircleville; it's generally losses.

P. H. Harsha, Portsmouth: Corn is soft in our section. A margin of 5 cents a bushel would seem small this year.

C. M. Myers, Ludlow Falls: Corn harvested with machine binder is in poor condition.

T. B. Marshall, Kirkwood: The buyer who starts to taking 72 pounds of corn for a bushel will get left. In Illinois I have taken 80 pounds for the first month and then drop to 75 pounds. I am not in a hurry to handle new corn.

C. M. Anderson, Franklin: Corn is a month later than usual. This crop should not be handled on less than a margin of 5 cents. We have some complaints of moldy corn. It will be a hard crop to handle.

H. Kress, W. Middleton: Corn in our section cannot be handled before middle of November. Corn gathered by a binder is reported to be rotting and moldy. We should have a margin of 7 cents.

F. O. Diver, W. Middleton: I have taken in several jags of corn. Some of it in bad condition. We will need double the usual margin to come out even.

John Parent, Union City: Corn is in much the same condition as reported from other sections.

E. A. Brecount, Conover: We will have considerable soft corn. It is too green to handle. I told a farmer the other day that we would take in new corn after the holidays. Some of our farmers have a box on the side of their wagons, into which they throw the soft ears. We try to encourage them to sort their corn carefully. We generally begin taking 75 pounds the first week, 73 pounds the next. We will need a wider margin than usual to come out even. We have no machine binders in our section. One corn shredder arrived yesterday, I am sorry to say.

J. G. Jones, Cleveland: I have spent the last two weeks traveling over the Big Four and examined the corn. Near Cleveland some corn was caught by frost and is in bad condition. Near Cincinnati the corn is in much better condition. Most of the dealers I have talked with do not intend to take in corn until about November 15. I have known of dealers taking 90 pounds to the bushel at the start and I think you wud need that many pounds at present to come out even.

A. A. Kuhl, Baltimore: I have traveled over Illinois and Indiana recently. The opinion of the dealers is that corn cannot be safely handled until the first of the year. You should buy on a margin of 5 to 8 cents. I think we will have a premium on cash corn until the middle of January. I believe shippers will make a mistake to sell new corn on track this year.

A. M. Rawn, Toledo: I represent that side of the trade supposed to be bulling the markets. I think if you will buy corn right, you can buy it and be safe from loss. In order to get a profit on any grain you must buy it right. Know what you are doing, what you can afford to do. Do not pay too high price because your near competitors do so. It will not bring you profits. New corn is in bad condition, very soft. Yesterday I saw corn near here as green as in August. It will not be ready to cut for three weeks. In some sections I have found small patches of dry, ripe corn, most of it is in very bad condition and I think shippers should be very careful.

E. W. Seeds, Columbus: Another reason occurs to me why we should be slow to ship new corn. The transportation facilities are so congested that no grain can be delivered within a reasonable time. This morning we received information regarding a car we shipped to Northeast, Pa., Aug. 19, which was not delivered until Sept. 21. I doubt if corn can be safely shipped until Jan. 1, unless the railroads can insure prompt delivery.

Percy Hynson, Columbus: In some sections the farmers are spreading out the shocks, in hope of getting it dry enough to husk. The transportation facilities are so badly tied up we must be very careful, lest we suffer heavy losses on hot corn.

President Grimes read the subjects offered for discussion at the afternoon session and announced that all would adjourn to the dining room for luncheon as the guests of the Columbus dealers.

THE LUNCHEON.

About 125 dealers were comfortably seated at a long L shaped table tastefully decorated with maidenhair ferns and candelabra. After a sumptuous repast the dealers returned to the meeting room and the program.

AFTERNOON SESSION.

The afternoon session was called to order by President Grimes at 2:10 o'clock, who said: During the noon hour I appointed a committee, of which Mr. Grubbs was chairman, to draft a resolution to voice the sentiments of the association on the morning's subject.

The Secretary will read the resolution. Secretary McCord read the following resolution, which was adopted:

Resolved, that the Ohio Grain Dealers' Association request dealers not to receive or contract new corn from the farmers until after they are fully satisfied it is in condition to ship, and that the secretaries of the different local associations be notified and requested to advise all members to this effect.

E. A. Grubbs, Chairman.

Secretary McCord read the following communication from C. B. Jenkins of Marion:

Marion, O., Oct. 7, 1902.
Mr. J. W. McCord, Columbus, O.

Dear Sir:-

In view of the fact that I will not be able to be with you at the State meeting on the 14th, all of which I very much regret, I thought I would write you my reasons and let you know that I will be with you in spirit if not in person.

Since the last meeting of our local asso-

ciation, I have taken new courage in association work. I am fully of the opinion, and it was so clearly expressed at our last meeting that, if we had not had a thorough organization and worked upon a uniform basis with a margin in view, the result of the oats crop would have been disastrous to many grain men. As it is nearly every one made money and are feeling good over the results of the organization.

I wish to convey through you to the members of the State association my earnest desire, as their Vice-President, that they take an active interest in establishing local organizations where they do not yet exist. It is a fact that some one must sacrifice a little in order to establish these organizations, but in the outcome it is worth all it costs and I believe that the time is near at hand when the organization of grain men will be so thorough that the questions which usually arise to mar and disturb the business will be lessened and quickly adjusted.

With my best wishes for a successful meeting, I beg to be remembered to all of the boys, and remain,

Yours truly, C. B. Jenkins.

He also read a communication from the Secretary of the Grain Dealers National Association regarding the Kansas Association.

E. W. Seeds: I move that a committee of two be appointed to take subscriptions.

P. R. Hynson: I move that the motion be laid on the table. Lost.

Mr. Seed's motion was carried.

The President appointed E. W. Seeds and C. E. Groce to receive subscriptions.

E. W. Seeds: A friend of mine who came from New England recently said the thing talked about most was western shippers' weights. Your weights are now guaranteed to within one per cent. I believe it would be to your advantage to reduce this to one-half per cent.

Secretary McCord presented the card reproduced on this page to be hung from roof of grain cars between doors of each car sent east.

E. A. Grubbs: Last summer when down East I found that some dealers would not buy from us because we would guarantee only to within one per cent of shippers weights. I move the adoption of the card read by this association. Seconded.

Mr. McCord: I think the use of the card would protect the shipper. The per cent of loss must be fixed later.

P. R. Hynson, Columbus: I think it would be well to send a duplicate of the card to the buyer, then we will have the same information as goes with the car. I wish to add to that motion that a copy be sent to the buyer.

A. E. Clutter, Lima: I think we should have the date of arrival reported.

The motion was carried and the card adopted.

E. W. Seeds: I move that it is the sense of the Ohio Grain Dealers Association in convention assembled that we guarantee the weights on grain to interior points to within one-half of one per cent. Seconded and carried.

A. E. Clutter: I wish to explain why I did not vote. I guarantee my weights in full. I want the highest price obtainable.

President Grimes called for views on the relations between millers and shippers.

F. Williamson, Monroe: I think the miller should buy from either man when he can.

P. H. Harsha: I find that the dealer mixes in poor wheat without saying anything about it.

J. S. Dewey: We buy from the farmer at our own station, but elsewhere we buy from the grain dealers.

C. P. Bauman, Winchester: I am waiting to hear from the shippers. I am both a miller and shipper.

Wes Hardman, Cable: In my locality the millers pay 2 to 4 cents more than I can afford, so they get the good wheat and I get the poor stuff.

E. M. Fullington, Marysville: If the shippers will co-operate with the miller and let them have wheat at the market price when they need it, both will be better off, and each will make a profit.

Fred Mayer of Toledo read the following paper:

Selling of 3 Red in Toledo.

Mr. President, and Gentlemen: In answer to that part of your secretary's circular, as to what action you should take with reference to Toledo dealers applying the maximum difference in price between No. 2 and No. 3 as a dis-

NOTICE!

This Car, initial No. ,

was Loaded with on

190 , containing Lbs.

In case of Loss or Damage, Railroad Agent must be notified at time of unloading. Claim must be accompanied by Paid Freight Bill, affidavit of weight and the following report:

Marks on seal, and whether broken or intact.....

Condition of grain doors.....

Condition of car floor.....

Condition of roof.....

Condition of body of car.....

Date of arrival.....

This Car was shipped by a member of the Ohio Grain Dealers' Association, affiliated to the National Grain Dealers' Association.

Form of Card Adopted by Ohio Association to Be Attached to Grain Cars.

H. W. Robinson, Greenspring: In Northwestern Ohio millers pay the same price shippers do and when millers need more than they get, they pay the shippers a profit of 2 cents a bushel.

President Grimes called for a discussion of what action should be taken with reference to the unfair grading in Toledo, placing 58½ to 60 pound slightly shrunken wheat in No. 3 grade, and applying the maximum difference in price between No. 2 and No. 3 as a discount.

I will call for Mr. Mayer of Toledo.

Fred Mayer: Our chief inspector is here. I prefer he would talk to you.

E. H. Culver, Chief Inspector, Toledo: I have samples of our wheat grades here, which I have submitted to different members here to-day, and they have agreed with me in the grading.

I do not think your accusation against Toledo grades is fair. You work a great injustice to the fairest market in this country. If you would come to our market and see how we strive to protect the shipper you would not have a kick. You are all invited to come to Toledo and see how we handle your grain. Have your Secretary come to Toledo and inspect our system and our inspectors. See if they are honest. We bulk our 2 and 3 wheat and average it.

count, will say that if your association can suggest a better plan for the selling of No. 3 Red than now prevails there we should be very glad to have you do so.

It is the custom in Toledo, as you all know, to sell all No. 3 Red (as well as No. 2 Red) by the grade—not by sample, and we at Toledo think this is a much better way than if each car of No. 3 Red was sold by sample. Some cars are poorer than others, but the difference in the quality is not so very great. No. 3 Red always rules on a supply and demand basis, whereas frequently No. 2 Red rules on a speculative basis. This causes the changing of the difference between the two grades.

If there is only a moderate amount of No. 3 Red it sells quite frequently the season through as close as 1½c to No. 2 Red. Where the crop is of poor quality and No. 3 Red is plentiful, it sells at a larger discount. The seaboard is Toledo's best buyer of wheat, and this season they have been taking mostly what is called "Combo" cargoes, meaning 1-3 No. 2 Red and 2-3 No. 3 Red, and the price that the seaboard was paying for this wheat is what made the price of No. 3 in Toledo. We grain men are always glad when the differences between off grades are narrow, because when that is the case shippers seem more satisfied, and it is pleasanter to do business. We certainly don't like to have our mail full of "Kicks" every day. I claim that the system of selling No. 3 Red in Toledo is better for shippers than is the system in vogue at Chicago, simply because shippers know every day just what they can expect for No. 3 red in Toledo, whereas in Chicago they don't. When the farmer has No. 3 red to sell you show him

the quotation of Toledo No. 3 red. If the No. 3 Red was sold here by sample there would be several different prices, and of course the farmer would claim that his No. 3 Red was of the highest grade, and would expect you to pay him on that basis. None of them would have poor No. 3 Red, and you would have more trouble than you are now having. There would also be more or less dissatisfaction on the part of shippers, many of whom claim their wheat is always the top grade when it is not. The grade of No. 3 Red in the different elevators here will almost run uniform. It is made up of many cars that are poor and many that are good, but a sample drawn from a 50,000 lot loaded out of any elevator will run almost alike, and the seaboard knows just what they get when they buy Toledo No. 3 Red. The high prices prevailing in Toledo now for 2 and 3 Red shows that the custom of putting all wheat of those grades together, is a benefit to shippers.

Sec'y McCord: This question was put on the program at the request of several members. I doubted the expediency of the action, but I think Chief Inspector Culver has fully satisfied everyone. I have a resolution here, which I will read.

Upon motion, the following resolution was adopted:

Resolved, that the thanks of the Ohio Association be extended to Toledo's Chief Grain Inspector E. H. Culver and Fred Mayer for their full explanation of the subject on the program referring to Toledo grades and prices.

A. E. Clutter: I think we should have a paid man of ability here in Columbus to watch out for vicious legislation which may prove detrimental to the grain business. Our experience with the bill providing for licensed engineers for plants of 10 h. p. up causes me to feel that we should keep on guard.

President Grimes called upon Mr. McAlister for views on the subject, "What action should be taken with shippers, track buyers, commission men or receivers, either members or non-members of this association, who continue to handle the business of scoop shovelers, farmers, and market disturbers after having been informed of the first offense?"

J. P. McAlister, Columbus: I think the regular dealer should be protected and no help given the scooper.

C. H. Tingley, Columbus: The handling of the scoopers rests largely with receivers and track buyers. If they continue to buy from the scoopers after having been informed, they should be treated accordingly. The members of this association should see to it that the track buyers are also informed of elevator men who are scooping at other stations. These disturbers merit squelching. We will not handle their business.

E. F. Sherman, Edison: At a station five miles from my elevator are two buyers who have no storage facilities except cars—when they get them. I have been asked to buy at that station and about concluded to do so. According to Mr. Tingley, I would be considered a scoop shovel.

H. W. Robinson: I think this scoop shovel trouble should be left with the local associations. I think we can handle this matter if we stick to our own territory. We had a scooper, but put up the price and let them have enough grain to swamp them. All regular dealers in our territory are members of our local association.

E. W. Seeds: I do not think you can find parallel cases. There are none. I believe the Washington C. H. shipper is not like any other on earth. He attempted to go through bankruptcy, but failed.

J. S. Dewey: I move that difficult cases be taken up by the executive committee

and the secretary be instructed to inform the track buyers what the disturber is doing. Carried.

E. F. Sherman: I move that the Ohio Grain Dealers Association do not recognize as regular any dealer who has not proper facilities for handling grain. Carried.

President Grimes called for the discussion of "Non-fulfillment of Contracts: Remedies, Adjustments and Recourse."

J. B. Seymour, Kenton: I have about quit contracting grain with the farmer. If the market goes up, I get about 50 per cent; if it goes down, I get 125 per cent.

C. F. Barnthouse, Morral: In my travels about I have noticed that there is too much indefinite in the dealings with the farmer. When the farmer comes in to learn the price of grain, I tell him, but before he departs I insist upon knowing whether he will sell to-day or take the market when he brings it in.

A. E. Clutter, Lima: Last August many dealers bought oats and sold them before the rains, then could not make deliveries. I think we should cease buying so far ahead. During the rains last summer many farmers wanted to sell their oats, but we refused to buy any until the threshing machines commenced work.

H. S. Grimes: I have a book in which all agreements to buy are written and the farmer signs it. I contract for a specific number of bushels, but agree to give the market for any excess at the time of delivery.

C. F. Parks, Arcanum: If shippers will put Quick Dispatch cards on their shipments, they are likely to reach destination more promptly.

O. P. Cheney, Canal Winchester: I have large cards printed "PERISHABLE—RUSH." I have tacked these to both sides of cars and it had absolutely no effect. In fact, I have cars so marked, out 38 days.

C. H. Tingley: I think it would help us if the Columbus shippers be permitted to tack such cards to cars containing corn from now until Jan. 1. It could do no harm.

E. W. Seeds, Columbus: Perishable freight takes a higher rate and I doubt if the railroads would permit of the deception. I would suggest that the cars be marked "NEW CORN, PLEASE RUSH."

E. H. Culver, Toledo: New corn from the Southwest is marked "NEW CORN, PERISHABLE, RUSH."

A. E. Clutter: I think that the Central Traffic Association is now considering the advisability of favoring the quick transportation for new corn. An order to that effect may be adopted. I think we should take some action.

E. A. Grubbs, Greenville: I move that the president appoint a committee of two to take up this matter with the railroads regarding the prompt forwarding of new corn. Carried.

The president appointed Mr. Grubbs and C. H. Tingley of Columbus.

J. S. Dewey, Blanchester: I have a resolution I wish to present.

Resolved, that the thanks of this Association be tendered to the dealers and track buyers of Columbus for the entertainment at luncheon to-day of those in attendance at this meeting.

A. E. Clutter, Lima: I think we should not be required to guarantee our grain to any destination. We bill it to Altoona or some other point for rebilling. With new corn we cannot afford to take such indefinite risks. If corn was dry, it would be different.

O. P. Cheney, Canal Winchester: I

sold corn for delivery Boston. The Boston buyer resold it to go way up in Maine and when it arrived there it was reported in bad condition. The Boston buyer came back upon me, but I denied liability. I did not sell for Maine delivery.

J. W. McCord: Grain sold to Columbus buyers is generally sold Columbus for rebilling. If shippers could tell just the day the grain would be started forward we could give direct interior billing, but sometimes it is one day, sometimes ten, before it is started.

The secretary explained the work of the American Farm Co. in the state, the action of Attorney General Sheets to oust the company. Damage suits have been commenced against several of our members by the American Farm Co., which will involve them in expensive litigation. I believe we should not permit all of this burden to fall upon them. They have begun suit for damages against the Marion Milling Co., the Marion Mirror and J. Gundy at Carroll. The farmers who subscribed for stock in the company want to get out and will be glad to contribute to the expense of the suits.

C. F. Parks, Arcanum: If shippers are required to load cars to capacity it will cause us much trouble. I understand the roads propose to do this. I believe we should do something to check this tendency. This applies to grain loaded for export.

C. M. Myers, Ludlow Falls: We order cars of stipulated capacity and if we do not get what we want, we get permission to load only to the capacity ordered. Thus we avoid overloading buyers.

A. A. Kuhl: We have had this matter up at Baltimore and hope to get relief. The Pennsylvania is the only road rigidly enforcing the rule, so far as I know.

Upon motion by Mr. Chaney President Grimes was added to the committee appointed to confer with the traffic association.

President Grimes: The term of office of the committee on arbitration has expired, so with their permission I will re-appoint the old committee: H. C. Wagner, Pleasant Corners; M. A. Silver, West Jefferson; J. H. Motz, Brice.

Sec'y McCord: Concerning our insurance company, will say we now have about \$335,000 in force. We have had another loss of a trifle over \$1,000. Business is coming to us faster than we can take care of it. All of our surplus we are turning over to the new Grain Dealers Mutual Fire Ins. Co. and other elevator and mill mutuals, which write \$5,000 on a risk, while we only write \$1,500.

President Grimes: I wish to thank our members for staying here and taking care of this business. I recognize the temptation to get off in the corner and talk with your brother dealers. I think the social feature is one of the most beneficial.

Secretary McCord: I wish to impress upon our members the need of their working to extend our membership. Some who come here year after year under the impression that they are members have not paid dues for years. Let each one try to bring at least one new member.

The convention adjourned.

CONVENTION CULLINGS.

The committee raised \$57.50 for the Kansas association.

Mr. Seymour rushed home to gather his onion crop—60,000 bushels.

One of the best attended meetings the association has ever held.

Baltimore was represented by A. A. Kuhl of G. A. Hax & Co.

Cincinnati's only representative was "Uncle John" Wooliscroft.

Toledo was represented by E. H. Culver; C. Knox, with Reynolds Bros.; Fred Mayer of J. F. Zahm & Co.; A. M. Rawn, with J. J. Coon; L. A. Mennel of Isaac Harter Co.

Cleveland's delegation included J. G. Jones, with the Cleveland Grain Co., and H. M. Strauss of Strauss & Joseph.

Fred Mayer gave away leather-bound pocket note books with the compliments of J. F. Zahm & Co. and Big Ed Culver gave each one of Toledo's handsome badges.

Columbus dealers in attendance included S. L. Douglass, T. R. Herr, P. R. Hynson, J. W. McCord, J. P. McAlister, H. L. Robinson, E. W. and K. B. Seeds, J. J. Stevenson, E. A. Storer, C. E. Switzer, C. H. and A. S. Tingley, E. R. Woodrow, G. M. Williams, E. W. Scott.

Two machinery men present—A. S. Garman, representing the Huntley Mfg. Co., and C. G. Smith, representing the Cleveland Elevator Bucket Co.

One Chicago dealer—Henry L. Goeman. Secretary Sampson of the Indiana Grain Dealers Association brot his better half.

Railroad men were scarcer than usual, but Huntington Fitch and A. L. Gilmore were there.

The Grain Dealers Journal was represented by Charles S. Clark.

L. G. Anderson's Sons, Franklin, and Kyle & Williamson, Monroe, were admitted to membership.

Among those present were C. M. Anderson, Franklin; M. J. Baker, Monroeville; C. F. Barnhouse, Morral; T. W. Baum, Duval; C. P. Bauman, Winchester; T. T. Beatty, Rattlesnake; D. W. Burgett, Columbus; T. G. Brawley, Greenville; D. Burns, Rossville; E. A. Brecourt, Conover; T. W. Brown, Circleville; B. Cain, Kirkersville; R. G. Calvert, Selma; O. P. Chaney, Canal Winchester; J. W. Channel, Melvin; R. F. Chenoweth, London; A. F. Cline, Mansfield; J. S. and L. W. Dewey, Blanchester; F. O. Diver, Middletown; O. Edwards, Troy; Z. Felger, Lewistown; E. M. Fullington, Marysville; J. Gundy, Carroll A. Gregg, West Jefferson; H. S. Grimes, Portsmouth; C. E. Groce, Circleville; E. A. Grubbs, Greenville; Wes Hardman, Cable; P. H. Harsha, Portsmouth; J. E. Hastings, Cedarville; H. S. Heffner, Circleville; A. Howard, Milford Center; C. E. Hummel, Bookwalter; W. J. Jenkins, St. Paris; Willis Jones, Mt. Sterling; J. R. Johnson, Baltimore; W. H. Johnson, Huntsville; W. T. S. Kile, Kileville; H. Kress, Middletown; G. W. Lamb, Hooker; F. E. Langdon, Wilmington; T. B. Marshall, Kirkwood; J. B. Miller, Richmond; S. R. Mitchell, Wilmington; J. H. Motz, Brice; S. A. Muff, New Carlisle; C. M. Myers, Ludlow Falls; Ed McCue, Pittsburgh; C. F. Parks, Arcanum; W. O. Patty, Fletcher; T. N. Patterson, Picketon; G. N. Perrill, Bowersville; C. O. Peters, inspector for the Ohio Grain Dealers Mutual Fire Insurance Association; D. B. Peters, Galloway; C. T. Pierce, Defiance; J. E. Pierson, Condit; W. H. Riddle, Sedalia; H. W. Robinson, Greenspring; J. Rowe, Verona; J. B. Seymour, Kenton; E. F. Sherman, Edison; M. A. Silver, West Jefferson; W. Slusson, Piqua; R. D. Sneath, Tiffin; C. M. Smith, Arcanum; R. Smith, Swanders; W. R. Sterrett, Cedarville; C. C. Taggart, Plain City; G. P. Teegarden, Ashville; J. S. Vent, Sedalia; J. B. Van Wagener, Lon-

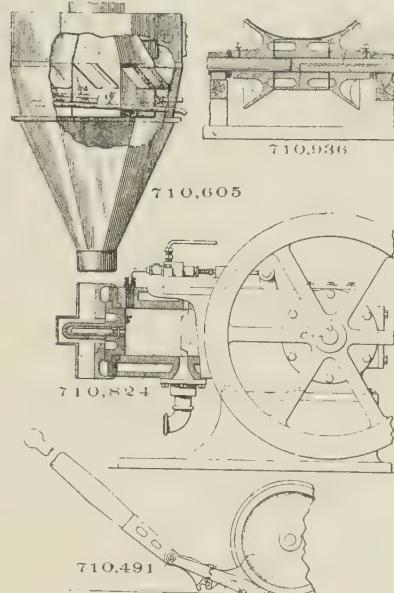
don; H. C. Wagner, Pleasant Corners; P. Werts, Verona; F. Williamson, Monroe; J. Wren, Deunquat.

Moisture in New Corn.

Corn of all sections may not contain as much moisture as that found in Ohio, but if it does all corn shippers must be very careful lest they suffer heavy losses from hot corn.

Secretary J. W. McCord of the Ohio Grain Dealers Association has recently conducted some interesting tests to determine the amount of moisture contained.

On Oct 15 he selected three ears of corn representing an average in quality and condition of central Ohio corn and placed them where they would encounter a current of air under natural conditions.



When first weighed, on Oct. 15, the three ears weighed 48 ounces; Oct. 16, 44 ounces; Oct. 17, 42; Oct. 18, 41; Oct. 20, 40; Oct. 22, 39. On close examination of the kernels they still showed too much moisture and not enough shrinkage to guarantee the shelled corn, if loaded in bulk, to continue sweet and cool more than four to six days.

During the same time a large ear of replant corn, very heavy and green, weighed under the same test on Oct. 15, 15 ozs.; Oct. 16, 14; Oct. 17, 13; Oct. 18, 12½, and Oct. 22, 11 ozs.

It is generally accepted that 90 to 100 ears of average size will make a bushel of ear corn. Eight such ears received from the fields on Oct. 22 after being indoors 24 hours weighed 112 ounces, showing that 100 ears would weigh about 84 pounds. Dealers who begin buying 75 pounds for a bushel will soon find their working capital shrinking as badly as the new corn.

Book Received.

THE REPORT of the Kansas State Board of Agriculture for the quarter ending September, is a presentation of the claims, merits and performances of the breeds of hornless cattle. Illustrated; 189 pages.

Contract grades for delivery on option contracts have been altered recently by the London Produce Clearing House, to include yellow La Plata maize, effective on October delivery.

PATENTS GRANTED

Edgar Byars, Rhome, Tex., has been granted letters patent, No. 711,097, on a baling press.

Edward G. Shortt, Carthage, N. Y., has been granted letters patent, No. 711,235, on a gas engine.

William J. Wright, Pittsburg, Pa., has been granted letters patent, No. 711,454, on a gas engine.

Robert L. Woodruff, Winder, Ga., has been granted letters patent, No. 711,270, on a hay press.

Walter G. Wilson, Westminster, Eng., has been granted letters patent, No. 710,727, on an explosion engine.

Herman E. Ebbs, East Nuremberg, Germany, has been granted letters patent, No. 710,911, on a gas engine cylinder.

Alanson P. Brush, Detroit, Mich., has been granted letters patent, No. 710,840, on a valve for gas or gasoline engines.

Patrick F. MacCallum, Fairbank, Scotland, has been granted letters patent, No. 710,483, on an internal combustion engine.

Louis W. Witry, Waterloo, Ia., has been granted letters patent, No. 710,647, on a speed regulator for explosive engines.

Wm. O. Worth, Chicago, Ill., has been granted letters patent, No. 710,728, on a valve and valve mechanism for explosive engines.

Theodore S. Glover, East Norwalk, Conn., assignor of one-half to Cyrus E. Crabbe, Stamford, Conn., has been granted letters patent, No. 710,771, on a spark-ing device for explosive engines.

Earle C. Bacon, New York, N. Y., has been granted letters patent, No. 710,936 (see cut), on a roller support for conveyor belts. The roller is mounted loosely on a shaft and between two conical pulleys fast on the shaft.

Carl W. Weiss, New York, N. Y., has been granted letters patent, No. 710,824 (see cut), on an incandescent igniter for explosive engines. Secured to the cylinder head is a dash plate upon which oil is discharged, and in which is mounted an independent tubular igniting tube open only at its forward end.

Richard Miller, Appleton, Wis., has been granted letters patent, No. 710,491 (see cut), on a car mover. The main lever is provided at its forward end with a segment gear engaging a corresponding segment gear pivoted on the pinch bar. The segment lever pivoted on the pinch bar bears against the car wheel and is given a powerful forward movement when the handle is depressed.

Wm. S. Osborne and Elwin C. Bryant, St. Louis, Mo., have been granted letters patent, No. 710,605 (see cut), on a dust collector. A polygonal casing has an enlarged head and a conical lower portion. Air entering thru an inlet in the head passes around the inclined portion of the hood in a circular direction and downwardly therefrom. Communication between the chamber in the head and the enlarged chamber in the conical portion is established by deflector pipes. Fluid is introduced into the apparatus by a perforated pipe.

An Indiana farmer recently weighed a bushel of corn in the ear, 70 pounds, and packed the ears in a box, which measured 4,446 cubic inches. This test shows that a little more than 2½ cubic feet are required to the bushel of corn in cribs.

GRAIN TRADE NEWS.

CANADA.

John C. Smith will succeed the late Lewis H. Robinson as manager of the Grand Trunk Elevator at Sarnia, Ont.

The Farmers Elevator Co. has begun the erection of a 35,000-bushel elevator at Grenfell, Man. Nearly 800,000 bushels of grain were shipped from Grenfell last season.

The Northern Elevator Co. has been appointed agent of the Canada Linseed Oil Mills Co., Ltd., and will purchase flax for the company throughout Manitoba and the Northwest Territories.

The Canadian Midland Linseed Oil Co., recently incorporated at Winnipeg, Man., will probably build a terminal elevator during the early part of next year and erect an oil mill in connection.

Fire broke out Oct. 10 in the elevator of the Hedley Shaw Milling Co. at St. Catharines, Ont., and for a time threatened to destroy the entire plant. The loss, including 20,000 bushels of wheat, is about \$14,000.

Commissioner Castle, Chief Inspector Horn, Secretary Bell and Robt. Muir of Winnipeg have complained to the Canadian Pacific Railway, alleging that wheat sent to King's elevator at Port Arthur, Ont., is cleaned 1 or 2 per cent, when the inspector specifies only $\frac{1}{2}$ per cent.

The Frontenac Cereal Co., Ltd., has been organized at Ottawa, Ont., with \$600,000 capital stock and has purchased the two plants of the Frontenac Milling Co., Ltd., and the Kingston Elevator & Transit Co. Those interested are Henry Moers, Kingston; Geo. Richardson, Kingston; Thos. Donnelly and the Hon. Wm. Harty.

The Dominion of Canada will have a special pavilion of its own at the St. Louis World's Fair, and will be represented in the principal departments. About \$125,000 will be expended on the exhibit by William Hutchinson, Canadian Commissioner. The agricultural resources of the Dominion will be prominently exhibited.

H. R. Patriarche, manager of the new car service and freight inspection bureau at Winnipeg, Man., has nothing to do with the distribution of cars. A new set of rules governing the right of shippers to hold cars will be enforced. For grain cars held longer than 24 hours the charge will be \$1 per day. This will not affect the man with modern facilities.

Arthur St. Laurent, the engineer in charge, reports that the work on the elevator now being constructed in the harbor at Montreal is progressing satisfactorily, and on Oct. 1 the pile foundations were complete, eight grain bins had been commenced and the steel was ready for 30 more. The Steel Storage & Elevator Construction Co. has the contract.

Receipts and shipments at Fort William, Ont., between Sept. 1 and Oct. 15, are almost double the quantity that there was during the same period in 1901. There have been no delays and no complaints at this end in regard to not getting enough wheat. Elevators A and B are about full now but C is empty, so there is no possible chance for a blockade.

Mr. McNamara, a workman employed

at the steel elevator which is now under construction at Fort William, Ont., was caught under a falling smokestack Oct. 20 and received injuries which resulted in his death. The smokestack was supported by a guy rope tied to a seldom used switch. A derrick which was being pushed across the switch on a flat car, caught and cut the rope, letting the smoke stack fall.

WINNIPEG LETTER.

H. Kirk of Gretna has applied for membership in the Winnipeg Grain Exchange. He is a well known private grain dealer.

Harry Dunsbith has been appointed manager of the Clearing Association for Winnipeg and the option board will be opened Oct. 27. The trading hours are from 9:30 to 1:30.

Jos. King of King's Elevator, Port Arthur, was in Winnipeg this week. He states that a linseed cleaning plant will be established at this elevator.

A delegation from the Brandon Board of Trade was in Winnipeg this week to interview General Superintendent Leonard of the Canadian Pacific, in regard to car shortage and freight charges.

Thos. Morton, M. P. P., of Gladstone, Man., is establishing an office of his firm in the Winnipeg Exchange. The firm of Morton & Pierson is one of the oldest in the Gladstone district.

Inspector Horn's statements show that a total of 9,784 cars of wheat alone have passed through Winnipeg to the lake front or via Emerson to Duluth. This means about 8,500,000 bushels since the crop commenced to move.

The Canadian Northern has announced that it is willing to ship wheat to Duluth and already about 100 cars have been delivered to the Northern Pacific. The cars of the latter company are used and emptied and each day are being brought into Manitoba to be sent via Emerson across the line.

The Lake of the Woods Milling Co. intends to erect a large mill in Manitoba. It now has a large mill in Keewatin, Ont. The cost of the new mill will be large and when completed will be the largest the company owns. The site has not yet been selected but will be at a railway crossing.

Flaxseed is selling at Winnipeg at \$1.08, lower by 12 cents than at Duluth or Minneapolis. The reason for this difference is that there is very little home market for the product and a market has to be found in England or in the East. The duty on flax is 25 cents per bushel and the freight to Minneapolis is about 15 cents, so that it would not pay to ship across the line.

Up to date there have been about 12,000,000 bushels of wheat marketed along C. P. R. points and threshing in many districts will be over in about a week's time, with a continuation of fine weather. The place where congestion seems to be worse is in the Deloraine district, which includes Medora, blocked since Sept. 15, Deloraine, Boisbavain, Holmfeld, Cartwright and about twelve other points. One company states that out of 60 elevators which they operate, twelve are now

filled to the roof and they are unable to get cars to ship wheat out.

Complaints of car shortage and blocked elevators are now so numerous that one gets tired hearing them. The trouble does not appear to be so much a shortage of cars as a blockade at the head of the lakes. Only two of the Canadian Pacific Elevators are operating, Elevator "D" not yet being completed. Throughout the country there is also complaint of car shortage but statistics of the wheat movement show that amount taken from the head of the lakes is so small compared to the amount moved that this end of the line is more to blame. Very little more than 2,500,000 bushels out of about 3,000,000 bushels of wheat handled has been shipped by vessel. The reason of this is hard to say unless, it is that shippers waited to complete their vessel space.—S. M. F.

CHICAGO.

Memberships in the Board of Trade are selling at \$3,750.

J. J. Badenoch will erect a one story, brick elevator at 926-38 West 17th St., to cost \$30,000.

Friends of Supervising Inspector Smillie are glad to learn that he is able to be out, after his three months' illness.

Mary Hilliard and Pauline Sturgis ran an oats deal, but fell out on the division of profits, and now the latter has brot suit for \$3,000 margins.

The Equitable Agency Co. has been incorporated with \$2,500 capital stock to conduct a brokerage business. Incorporators: A. L. Johnson, Sanger Smith and R. K. Smith.

Charges that names of men no longer in the employ of the state grain inspection department at Chicago were still carried on the pay roll have been proved to be without foundation.

Robert Thorburn has been suspended from the Board of Trade for one year for dishonorable conduct, growing out of his firm's shortage in July corn and the failure to pay \$38,000 in settlement on defaulted contracts.

Fire and explosion on the morning of Oct. 22 destroyed part of the glucose works of the Corn Products Co. Several men were burned to death. While the main building is still standing the loss on the burned dry house, starch rooms and stock foots up about \$400,000.

Arthur R. Sawers recently distinguished himself at Columbus, O. Returning to his hotel from the theatre on the night of Oct. 10 he found the hotel on fire and the inmates scrambling out without their clothes. After saving one fair maiden from a dangerous position he gave her his overcoat, and to another gave his sack coat. Not every grain dealer has such an opportunity to be a hero.

H. B. Slaughter has reported that Bartlett, Frazier & Co. have shipped out grain stored in their own elevators and owned by them. As this is a technical violation Slaughter has seen fit to stir up the matter, much to the disgust of the other directors of the Board of Trade. Wm. Nash has refused to have anything to do with the investigation, and D. G. Richardson has recommended that the charges be dropped.

The directors of the Board of Trade have resolved that "Advertisements inserted, or caused to be inserted, in the public journals, by members of this board, pretending to predict the course of the markets or describing plans or schemes more or less plausible and sen-

sational by which money may be made, are devoid of any commercial character whatsoever, and are injurious to the good name and dignity of this association. That it is the sense of this directory that such advertisements are unbecoming a member of this board and are deemed dishonorable conduct within the meaning of section 9 of rule IV. of the rules of the Board of Trade."

ILLINOIS.

Isaac H. French & Co. have opened a grain office at Decatur, Ill.

The elevator at Mays, Ill., is receiving various repairs and new machinery.

Ralph J. Cocking has taken charge of an elevator at Alexis, Ill., for the Neola Elevator Co.

The Northwestern Elevator Co. proposes to move its elevator at Sterling, Ill., to a better site.

It is reported that Adolph Schumacher of the La Rose Grain Co. will build an elevator at Varna, Ill.

N. Bentz, Nokomis, Ill.: Inclosed find payment for advertisement of scales for sale. Money well spent.

E. R. Ulrich & Sons have recently installed a Boss Car Loader in their elevator at New Berlin, Ill.

S. L. Smith of Raven, Ill., has no facilities for shipping grain, but manages to do it by means of the shovel.

C. H. Albers of St. Louis has purchased land in East St. Louis, Ill., for the erection of an elevator 100 feet square.

Mattoon Elevator Co., Mattoon, Ill.: Our grain dumps were sold thru our advertisement in the Grain Dealers Journal.

A car of new corn which was received Oct. 22 from Sterling, Ill., was graded no grade on account of moisture, and sold at 35 cents.

Mr. Appleton will manage the elevator at Casey, Ill., recently purchased by Buxton & Appleby of Mr. Austemiller. Harry Lewis will assist him.

Wm. Bousleth has purchased the elevator at McHenry, Ill., and will conduct a grain and feed business, besides handling machinery and operating a pickle factory.

Henry W. Hudson will be corn buyer for the American Hominy Co., with headquarters at Decatur, Ill. He was formerly with the Carrington-Patten Co. in the cash grain department.

The Sidell Grain Elevator Co. has been incorporated at Sidell, Ill., with \$50,000 capital stock to deal in grain. Incorporators: William G. Cathcart, John H. Herron and Ralph G. Herron.

The Farmers Grain Co. has taken possession of the Bowen Elevator at Garrett, Ill., with J. W. Laughlin as manager. Although but recently organized many of the farmers are dissatisfied and all kinds of trouble is looked for.

G. W. Treadway, a farmer living near Billett, Ill., is attempting to contract corn from farmers. He has not even a pair of scales and his only storage room is a box car. He also operates under the name of G. W. Treadway & Co.

Frederick A. Keener, who about 30 years ago engaged in the grain business in Illinois and operated a line of steamers on the Mississippi River, died Oct. 12, of heart disease, in Denver, of which city he had become a prominent citizen. He was 75 years old.

C. H. Ruple and Geo. A. Starz have purchased the elevator and business of Richardson & Hopkins, at Olson Station, Ill., and will continue the business after Nov. 1, under the firm name of C. H.

Ruple & Co. Mr. Ruple will remain at Chestnut and Mr. Starz will manage the business at Olson, with post office at Peru.

George Seeber, a farmer owning a large portion of the town of Condit, Champaign county, Ill., is building a large elevator on his property in section 13. The building will be completely equipped with machinery including dumps and conveyors. Mr. Seeber raises horses and the elevator will be used for feed storage.

About 25 farmers in the vicinity of Athens, Ill., are very much interested in the organization of a Farmers Elevator Co. Two sites at East Athens have been offered them and plans are being made for incorporation with \$5,000 capital stock. Some of those interested are Joe Graham, F. H. Whitney and John Grant. After running it a while the farmers will have another elevator for sale.

J. H. Dole & Co., owners of an extensive line of country elevators, have recently been making extensive improvements in many of their houses, equipping them with up to date gasoline engines, and have built an entirely new house at Meriden, Ill., re-built an elevator that was burned at Mendota, Ill., and contemplate erecting one or two more new houses before Christmas.

INDIANA.

Martin A. Current will rebuild his elevator recently burned at Stateline, Ind.

The Fisher Grain Co. of St. Paris, Ind., is building a dwelling house near its elevator.

J. A. Mouch of Mooreland has purchased the elevator of Frank M. Reece at Messick, Ind.

R. L. Bond is having his elevator at Oaktown, Ind., overhauled and new machinery installed.

Barnard & Fries of Greenfield, and H. M. Stehman of Cicero, Ind., have recently purchased Boss Car Loaders.

Frank Rich will operate the elevator at Goodland, Ind., purchased a short time ago by Rich Bros. of the Goodland Elevator Co.

Fred P. Rush, well known among the grain dealers of Indiana, has recently returned from a European trip, to his home in Indianapolis.

J. H. Hodapp has a large force of men at work on his elevator and corn mill at Seymour, Ind., and it will probably be completed within a month.

The Piel Bros. Starch Co. has been incorporated at Indianapolis, Ind., with \$500,000 capital stock, by Piel Bros., formerly interested in the National Starch Co.

The Matt Schnaible Grain Co. has put two dumps in its elevator at La Fayette, Ind., and is now installing an oat clipper. Better facilities ought to mean more business.

John Brodie of Shinkle's Station, Ind., shipped the first car of grain over the Cincinnati, Richmond & Muncie road Oct. 9. The 1,800 bushels of wheat were shipped to Pittsburg.

The finding of the master in chancery in the suit of the Chicago Board of Trade against L. A. Kinsey & Co., alleged bucket shop operators of Indianapolis, Ind., is a distinct victory of the Board of Trade, since the master ruled that the Board has common rights of property in its continuous quotations; and that the quotations are not affected with public interest to the extent of public control.

In regard to the elevator which was burned at Goodland, Ind., a member of

the Hartley Grain Co. stated that about 280,000 bushels of grain insured for \$47,500 were in store in the building, which, valued at \$30,000, was insured against fire for \$20,000. About 50,000 bushels of oats grading No. 4 will be saved. The elevator will be rebuilt at once on the old site, but probably not on as large a scale.

The Central Indiana Division of the state association held a district meeting Oct. 16 at Marion, Ind. About 40 dealers were present. Corn was reported to be a fine crop, but owing to the backward season it would not begin to be moved much before the middle of November, in the opinion of the dealers present. O. J. Thompson of Kokomo is the president, A. F. Files of Muncie secretary and Geo. Woods of Windfall treasurer. Among those present were: J. C. Davis, West Middleton; I. A. Adams, Bunker Hill; Henry Maxwell, North Waverly; D. A. Gillespie, Logansport; C. M. Custer, Logansport; John Winn, Lucerne; D. R. Ross, North Grove; George C. Woods, Windfall; A. F. Files, Muncie; J. M. Coucher, Bennetts; O. J. Thompson, Kokomo; E. J. Davis, Greentown; C. M. Barton, Kokomo; F. C. Davis, Swayzee; M. Garrison, Converse; T. D. Hurst, Toledo; H. M. Freeman, Sims; R. P. Henley, Swayzee; J. H. Price, Kokomo.

INDIANA LETTER.

Churchill & Co. are building a corn crib 24x30x30 in connection with their elevator at Baldwin.

J. F. Beard is building a hay and wool barn, 30x30 feet, which will be completed Nov. 25.

Frank Barna of St. Joe, Ind., is building a flour mill at Orangeville. He reports a small acreage of wheat going into the ground; been too wet.

The Northwestern Ohio & Eastern Indiana Grain Dealers Association met at Fort Wayne, Ind., Oct. 9, and held a very interesting weekly meeting. Many topics came up and were thoroughly canvassed to a successful end. A good social meeting was held; and just the kind of salve that is needed to heal wounds was applied very freely. Let all dealers attend these weekly meetings and get the good that results from these social chats. The Whitmer Grain Co. of Grabill and Kraus & Apflebaum of Columbia City, Ind., became members. Walk up, gentlemen, and help yourselves to the good things which you will find on tap in the association every working hour of the day. It can do you a world of good. Give it a trial. The association adjourned to meet Oct. 16 at Longwell Bros.' office, Van Wert, O. Ohio dealers present: W. C. Long, Convoy; De Witt Whittey, Oakwood; Thos. Singer, Dixon; H. C. Tinkham, Latty; Asa Smith and C. A. Bissell, Antwerp; Leroy Churchill, Toledo; P. Morgan, Venedocia; P. C. Miller, Celina. Indiana dealers: Joseph Whitmer, Grabill; C. G. Egly, Berne; C. E. Bash and J. F. Beard, Huntington; M. Yaggy and Ed Masher, Woodburn; E. L. Carroll, Decatur; A. Krauss, Columbia City.—C. A. B.

IOWA.

The Northern Grain Co. will build an elevator at Toledo, Ia.

The Northern Grain Co. has succeeded J. J. Goodall at Quigley, Ia.

D. J. Jenks & Son are remodeling their elevator at Williamsburg, Ia.

H. E. Gingery is putting a new dump into his elevator at Woden, Ia.

J. F. Vass has succeeded Vass Bros. in the grain business at Eldon, Ia.

C. L. Roberts has nearly completed his new grain office at Grinnell, Ia.

The Moerke Elevator at Wallingford, Ia., has been closed by the Bank of Wallingford.

The grain warehouse of Goerdt Bros. at Dyersville, Ia., was recently damaged by fire.

The Orchard Produce Co. will continue the business of John Robertson at Orchard, Ia.

The Oakland Grain & Coal Co. has succeeded the South Branch Elevator Co. at Oakland, Ia.

M. B. Lytle has succeeded F. C. Chinn as manager for the Des Moines Elevator Co. at Wiota, Ia.

The Neola Elevator Co. has succeeded H. A. Bechtel at Bayard and Smith & Milligan at Granger, Ia.

Rudluff Bros. will continue the business formerly conducted by Rudluff Bros. & Meier at George, Ia.

A. F. Froebel will continue the business formerly conducted by Froebel & Stoner at Tiptonka, Ia.

Fallgatter Bros. have purchased the grain and milling business of H. J. Benson at Marshalltown, Ia.

Counselmann & Co. are rebuilding their elevator at Gilmore, Ia., which was burned about a month ago.

W. Reynolds and family have moved to Alvord, Ia., where he will take charge of the elevator for G. H. Barber.

O. S. Hovland will take charge of the elevator belonging to the Nye-Schneider-Fowler Co. at Eagle Grove, Ia.

Ingalls Bros. of Wyoming, Ia., have purchased the elevator at Cambridge formerly operated by Johnson Bros.

The O. M. Worrell Lumber Co. of North English, Ia., has succeeded the Keswick Lumber Co. at Keswick.

A starch factory requiring 2,500 bushels of corn daily and costing about \$200,000 is to be erected at Cedar Rapids, Ia.

G. M. Gwynne, Essex, Ia.: While I am in the grain business, I shall always want the Grain Dealers Journal to come regularly.

Some chaff in the engine room of the Seavers Elevator at Davenport, Ia., caught fire Oct. 20. Slight damage was done.

C. T. Sidwell, Florence, Ia.: It was thru my advertisement in the Journal that I secured a buyer for elevator machinery.

D. N. Dunlap, Fontanelle, Ia.: Please discontinue by advertisement in the Grain Dealers Journal for man wanted. I am flooded with answers.

The Atlas Grain Co. has purchased the elevator and grain and coal business of S. S. Hanson at Collins, Ia. Mr. Hanson has retired to look after his farm and stock business.

The elevator of the Lehan Grain Co. at Dunlap, Ia., together with 8,000 bushels of grain and several cars of coal, was destroyed by fire Oct. 15. Loss about \$10,000, partly insured.

E. L. Dieckman, formerly manager for the Atlas Elevator Co., at Maurice, Ia., has been transferred to Ferney, S. D. Ben Bell, formerly at Struble, will succeed him at Maurice.

P. C. Hanson & Sons, McCallburg, Ia.: We find the Grain Dealers Journal a very instructive paper for those interested in the grain business. We take great interest in it and would feel lost without it.

Richardson & Co., of Chicago, have awarded the contract to the Younglove

& Boggess Co. for a 50,000-bushel corn elevator at Fort Madison, Ia., which will be equipped with a corn sheller with an hourly capacity of 1,000 bushels.

Geo. A. Wells, secretary of the Iowa Grain Dealers Association, was at Tipton Oct. 10 to meet C. H. Michel of Dixon, L. H. Damman of Bennett, K. K. Liquin of Clinton and S. Graham of Onslow to discuss matters pertaining to the grain business and association work in that section.

The elevator of the B. A. Lockwood Grain Co. at Ames, Ia., was destroyed by fire Oct. 20, causing a total loss of \$8,000, which is, however, entirely covered by insurance. The grain, valued at \$3,500, was mostly wheat and oats, but included 7,000 bushels of corn. The fire started in the cupola.

The Farmers' Elevator Co., of Whittemore, Ia., failed to convict Manager Thompson and their \$14,000.00 indebtedness can only be attributed to bad management. The farmers should learn by such experience that grain should be bought with a margin to pay expense and losses.—Iowa Grain Dealers Quarterly Bulletin.

A. E. Wells has purchased the interest of A. J. Wilson in the elevators at Laurens, Albert City, Havelock, Varina, Webb, Marathon and Curlow, Ia., and business will henceforth be conducted under the firm name of DeWolf & Wells, with headquarters at Laurens. The company also has control of the bank at Curley. Mr. Wilson will retire from the grain trade and go to Dakota where he will have charge of a large cattle ranch.

G. M. Gwynne, Essex, Ia., Oct. 9: Since my return from Colorado last week I have been daily visiting corn fields and making a study of crop situation. I find the frost about the middle of September did very little damage to corn; although it killed the foliage it left the stalks un-hurt and the cloudy, wet weather has kept the corn filling until we have a finely filled crop, whereas had the weather been warm and dry following the frost, half of the crop would have been very badly damaged; with Indian summer approaching and possibly a fine fall coming, we will have plenty of No. 2 corn by December; the talk of corn rotting and growing is only a bull croak.

Geo. A. Wells, secretary of the Iowa Grain Dealers Association, in his crop report issued Oct. 10, states that 13 per cent of the oats will grade 3 white, 51 per cent 4 white, 27 per cent no grade and 9 per cent not marketable. The marketable corn will be 74 per cent of an average crop. The total crops this year are estimated at 13,670,000 bushels of wheat, 121,745,000 bushels of oats, 13,687,000 bushels of barley, 884,786 bushels of rye and 768,110 bushels of flaxseed; compared with 18,295,000 bushels of wheat, 114,883,000 bushels of oats, 227,908,000 bushels of corn, 14,654,000 bushels of barley, 859,630 bushels of rye and 916,890 bushels of flaxseed, last year.

At a special meeting of the Cereal Club at Des Moines, Ia., Oct. 7, the following resolutions were unanimously adopted: Whereas, the Cereal Club was the recipient of special attention from the Chamber of Commerce organization of Peoria, on the occasion of the 7th annual convention of the Grain Dealers National Association, therefore be it resolved:

That the personal thanks of all members of the Cereal Club be extended to the members of the Chamber of Commerce, of the city of Peoria, with their

assurance of a sincere appreciation of their unfailing courtesy, which has helped to make their visit memorable.

That the personal thanks of all members of this club be extended to his honor the Mayor of the City of Peoria, W. F. Bryan, through whose hospitality we were permitted the use of the "Keys of the City," opening to our inspection the doors of the great distilling, glucose, implement, iron and steel interests, for which the city is famous.

KANSAS.

It is reported that an elevator will be built at Burden, Kan.

Irwin & Bunds are enlarging and remodeling their elevator at McLouth, Kan.

J. R. Wright has bought the grain business of J. C. Connell at McPherson, Kan.

Stuckey Bros. have bought the grain business of A. C. Eyman at Moundridge, Kan.

Steel & Son have purchased the grain business of E. R. Clark at Kansas City, Kan.

The Coffey County Lumber Co. has purchased the grain business of M. L. Hayes at Waverly, Kan.

The business men of Wichita, Kan., are striving to interest Frederick Seeley, a member of a grain and feed firm of St. Louis, in the building of an elevator at Wichita.

Hedge & Marshall are improving their elevator at Whiting, Kan., and have put in another stand of elevators, built a brick smoke stack and installed new hopper scales.

The case of E. J. Smiley, secretary of the Kansas Grain Dealers Association, has been appealed to the Supreme Court of the United States, the state supreme court having refused a new trial.

According to the report for the month of September, the state grain inspection department of Kansas has a balance on hand of \$23.75. Fees brought in \$3,137, while expenses amounted to \$3,113.25.

The Goffs Grain Co. has two elevators at Goffs, Kan., one on the K. C. N. W. and the other on the M. P. R. R. John McManis, who has been with the company for nine years, is manager of both elevators.

On account of domestic trouble, Frank Muck, engineer for the Elwood Elevator Co. at Elwood, Kan., took his own life by jumping into the Missouri River at St. Joseph, Mo., while on his way to work one morning.

KANSAS LETTER.

A. Johnson & Son are operating the elevator at Cleveland, Kan.

Hall & Robinson have succeeded Scott McCormick at Mt. Hope, Kan.

G. L. Horton & Co. are out of the grain business at Cullison, Kan.

S. R. Bagwell is no longer engaged in the grain business at Rossville, Kan.

F. H. Bennett of Topeka has returned from his extended visit to Salt Lake.

C. J. Kreisel of Olmitz, Kan., has disposed of his grain interests at that point.

Local meetings will be held this week at Marysville, Seneca, Sabetha, Atchison and Holton.

H. B. Dickerhoof has recently purchased the elevator of the Denton Grain Co. at Dentonville, Kan.

The Stafford Mill & El. Co. of Stafford, Kan. has changed the firm name to the Larabee Flour Mills Co.

The Turon Gr. Co. of Turon, Kan., has been succeeded in the grain business at that point by Price & Stevenson.

Gregg Bros., with headquarters at St. Joseph, have purchased the elevator formerly operated by Sherrard & Son at Oneida, Kan.

Mowrer Bros. have just completed their new elevator at Lost Springs, Kan., being compelled to rebuild, as their elevator was destroyed by a tornado last August.

The Central Granaries Co., with headquarters at Lincoln, Neb., have offered a number of their Kansas stations on the Oberlin branch of the Burlington for sale.

A. H. Bennett of Topeka, Kan., visited Washington, D. C., during the G. A. R. convention in that city, being a member of the Modoc Club of Topeka, Kan., who furnished a part of the music.

C. W. Hoyt, formerly a commission merchant of Topeka, Kan., has established himself in the grain business at Bartlesville, I. T., and is operating a number of tributary stations in that territory.

A very interesting meeting was held at Belleville, Kan. Those in attendance were T. D. Worrall of the Nebraska El. Co. of Lincoln, Neb.; E. A. Woodman, Mahaska, Kan.; M. P. Harrison, Belleville; Belleville El. Co., Belleville; Bert Ainsworth, Rydal; M. Hancock, Rydal; Thomas & Harrison, Athol; D. H. Ferguson & Co., Burr Oak; J. W. Pinney & Co., Scandia.

A very successful local meeting was held at Concordia recently, those present being R. H. Braden, Washington; C. W. Lawless, representing the Duff Grain Co. of Nebraska City; F. P. Lint of the Greenleaf-Baker Grain Co. of Atchison; W. S. Washer of the S. R. Washer Grain Co., Atchison; Murdock Grain Co., Clifton; W. Wangerein, Clifton; R. B. Miller, Clyde; G. M. Smith, Concordia; B. F. Hahn, Concordia; O. A. Seaton, Jewell City; Edwards & Son, Jewell City; J. W. Pinney & Co., Scandia; J. S. Hart, Randall; Burke & Co., Scottsville; John Charlesworth, Scottsville; W. L. Brandon, Clyde; O. B. Strom, Shurdahl; A. H. Pooge, Kackley; W. H. Beatty, Norway; H. C. Strohm, Waterville, and P. F. Lucas of Goffe, Lucas & Carkener, Kansas City, Mo.—A. M. D.

MICHIGAN.

The new elevator at Beaverton, Mich., will be equipped with an electric dynamo.

Geo. Jessup has put twelve new bean pickers in his elevator at Pompeii, Mich.

L. Frensdorf of Hudson has bought the elevator of Hadley & Marsh at Waldron, Mich.

Burglars broke into the elevator of Hart & Sullivan at Almont, Mich., Oct. 6, but had their labor for nothing.

Sachs & Wenzel have installed a 14-h.p. gasoline engine and feed mills in their warehouse at West Branch, Mich.

The Manna Cereal Co. has been incorporated at Detroit, Mich., with \$500,000 capital stock to manufacture a cereal food from wheat.

Fred Middlesworth, grain dealer at Linden, Mich., will engage in the wool trade and will erect a large elevator for his grain and wool.

Manufacturers of prepared foods are getting the bulk of the Canada pea crop of the Saginaw district, at 15 to 20 per cent higher prices than the seed trade are offering.

The Carpenter-Cook Co. has purchased the warehouse and grain business of Losselyong Bros., at Marquette, Mich. Losselyong Bros. still retain their feed business.

Bert Cripps received a prize of a set of harness for the largest load delivered at Wiesinger & Minson's elevator at

Deerfield, Mich. It contained 206 bushels weighing 8,275 pounds.

For three successive years the heaviest yields of grain in northern Michigan have been produced from emmer (spelt). This season's crop averages 60 bushels, and in some instances as high as 90 bushels per acre have been obtained.

H. E. Botsford & Co., Detroit, Mich., Oct. 10: Bean market has been on the rampage, advancing 65 cents a bushel in three weeks, owing to severe damage to the crop by excessive rains. Reliable authorities estimate the Michigan crop this year at 750,000 bushels against 4,000,000 in 1901.

O. W. Leöffler, Bad Axe, Mich., Oct. 18: Rainy weather has prevented sowing of the usual wheat acreage and only about two-thirds of a crop has been put in; bean crop will be small and of inferior quality on account of the rainy season; clover seed promises about one-half of a crop and that a poor article.

Fred M. Warner, secretary of state, Lansing, Mich., in his crop report issued Oct. 10, estimates the total yield of wheat in the state as 18,000,000 bushels. The yield per acre of oats is 38 bushels, and of corn 28 bushels. Continued rainy weather during September practically ruined the bean crop in most parts of the state. This is especially true in the counties that produce large quantities of beans. In some instances a part of the crop has been secured, but as a rule the larger portion was damaged so as to be fit only for feed. A few correspondents report that some farmers contemplate plowing under the whole crop, thus making a total loss. The estimated yield of beans per acre in bushels is, in the southern counties 10, in the central counties 12, in the northern counties 15 and in the state 11. The total acreage of buckwheat is 21,388, and the average yield per acre 14 bushels. During August and September farmers marketed 1,025,000 bushels of wheat, which is 203,400 bushels more than that marketed during the corresponding two months a year ago.

MINNEAPOLIS

Herman Groetsch of Gaylord, Minn., has entered the employ of the H. Poehler Commission Co. at Minneapolis.

Geo. J. Hammond says his new independent chamber of commerce at Minneapolis will open its doors in a few weeks.

The Albert Dickinson Co. will build a flax elevator on part of the land in Minneapolis recently purchased by Albert Dickinson.

R. C. Burdick, Minnesota's first grain inspector and until a year ago connected with the state grain inspection department, died Oct. 13, aged 69 years. He was a resident of St. Paul.

Blew, Armstrong & Co., dealers in grain and feed at Minneapolis, Minn., borrowed \$40,000 on forged drafts and bills of lading before their fraudulent operations were discovered by a draft coming back protested. Both are members of the Chamber of Commerce; but will be expelled if the jury finds them guilty.

That the campaign of the Minneapolis Chamber of Commerce against the bucket shops is striking the bucketeers in vital spots is evident from the howl of rage coming from the enemy's camp. Fearing that the Chamber will succeed in stopping the theft of the legitimate continuous quotations the captain of the bucket shops contemplates manufacturing his own quotations with a view to holding the trade of suckers, which last year is said to have

paid him a profit of \$200,000. The gamblers who choose to style themselves "independent brokers" threaten to organize a rival chamber of commerce, and build elevators in connection with the Northwestern Independent and Farmers Elevator Association, with headquarters at St. Paul.

In his annual address to the Chamber of Commerce, President John Washburn said: While we are a prosperous organization, there is one thing that menaces our prosperity. One of the worst features of the trade, and one which our board of directors have been taking measures to suppress, is the bucket shops. The bucket shop is bad. It is irresponsible. It is a menace to legitimate trading. It is offensive to responsible traders. It is a sore spot that must be removed by heroic measures. It has got to go, and we hope speedily. Some little headway has already been made by your board against this great evil, and the work must still go on. Great praise is due to the Chicago Board of Trade for their vigorous efforts against this uncommercial and dangerous institution, and the appreciation of all commercial bodies is due to the president of the Chicago Board of Trade for his unrelenting and vigorous warfare against bucket shops, which have long brought disrepute on all legitimate grain organizations. It is to be hoped that the legislature convening next winter will be urged to pass vigorous laws similar to the statute laws of the state of Illinois, against these uncommercial dealers who operate the bucket shops.

MINNESOTA.

Ben Boese is erecting a feed mill at Delft, Minn.

The Empire Roller Mill Co. will build a 50,000-bushel elevator at New Ulm, Minn.

E. J. Korte of Aurora, Minn., has purchased a gasoline engine for use in his feed mill.

J. A. Danielson will erect an elevator in connection with his warehouse at Lake Park, Minn.

H. M. Babcock is arranging for the erection of an elevator and feed mill at Dundas, Minn.

The Skyberg Farmers Elevator Co. has been incorporated at Skyberg, Minn., with \$5,000 capital stock.

Mr. Broughton of Garretson has succeeded Mr. Nerison as manager of E. A. Brown's elevator at Booge, Minn.

The Red Wing Linseed Mills have purchased the warehouse at Red Wing, Minn., of Fred Seebach, to use for storage purposes.

A 20,000-bushel elevator will be included in the 100-barrel mill plant which Henry Rippe is preparing to erect at Ceylon, Minn.

Mark N. Tisdale informs us that he has sold his elevator at Slayton, Minn., to Geo. R. McKittrick of Harmony, who has taken possession.

E. Blankenburg of Springfield, Minn., has secured a portable elevator which is run by a gasoline engine, to use in unloading wagons into cars.

Martin C. Paulson, who for ten years has been in the employ of the Hubbard & Palmer Elevator Co. at Mankato, Minn., died Oct. 11 of consumption.

A. J. Clark, Minneapolis, Minn., Oct. 19: Corn crop around Pipestone and Holland, Minn., very poor; frost has cut it off twice; crops around Stillwater, good.

W. F. Markham has purchased the Waldron Elevator of the Brooks-Griffiths

Co. at Sargeant, Minn. He still retains his elevator at Waltham but will reside at Sargeant.

MISSOURI.

W. A. Triplett has succeeded Triplett & Thrombro at Knob Noster, Mo.

E. D. Russell, East Prairie, Mo., Oct. 12: Corn in fine condition; wheat looks fine, large acreage sown.

Ziebold & Binz have succeeded Robert Womach in the grain business at St. Joseph, Mo.

C. D. Marshall of Brunswick, Mo., deals only in hay and seeds and does not pose as a grain dealer.

The Flanagan Mill & Elevator Co. has been incorporated at Rich Hill, Mo., with \$200,000 capital stock.

Secretary Stibbens of the Grain Dealers Union is making an effort to extend that organization so as to include all the progressive dealers of the state. The success of the union in the past should be sufficient to prompt every dealer of the state to avail himself of this opportunity to advance his business interests.

F. W. Bergerfer, one of the men interested in the manufacture of the cereal food called "Nutritia," is striving to interest the Retail Merchants Association of St. Joseph, Mo., in the erection of a factory in that city. The company now has plants at Battle Creek, Mich., and Toronto, Ont., and is arranging to erect one at San Francisco, Cal.

Geo. B. Ellis, secretary of the Missouri State Board of Agriculture, states that the acreage of wheat harvested in Missouri this season is 3,189,000, which is an increase of 51.4 per cent over 1901. With the average yield of 20.3 bushels the total product is 64,736,000 bushels. The acreage of corn is 7,645,000, but it is too early to estimate the yield, which, however, will be the largest on record.

NEBRASKA.

J. L. Sims has succeeded W. H. Harrison as grain buyer for the Duff Grain Co. at Danbury, Neb.

Erickson Bros. have repaired their elevator at Stromsburg, Neb., and put in an 8-h.p. gasoline engine.

Rankin Bros. are building more bins in their elevator at Cambridge, Neb., and installing a new feed grinder.

The Bassett Hardware & Supply Co. will engage in the grain business at Bassett, Neb., as regular dealers.

Quite a quantity of oats was stolen recently from a car near Norfolk, Neb., which belonged to the Omaha Elevator Co.

The Peavey Elevator Co. is building a large double corn crib, 100x32 feet, in connection with its elevator at Bancroft, Neb.

The Nebraska Elevator Co. has built large coal sheds at Rising, Neb., and is remodeling its elevator and fitting up an office.

A. J. Clark, Minneapolis, Minn., Oct. 19, writes: Crops near Bloomfield, Neb., fair; corn very good; had some heavy frosts last month.

J. Warren Keifer, Jr., has purchased the elevator at Bostwick, Neb., of W. H. Ferguson. After some repairing he will be ready to receive grain.

The Omaha Elevator Co., of Omaha, Neb., has increased its capital stock from \$2,500,000 to \$550,000. F. T. Heffelfinger is president of the company.

An elevator at Heartwell, Neb., on the B. & M. road, was recently run into by a

freight car and knocked from its foundation. It is being repaired.

John Tighe of Manley informs us that Dan Bourke of Bancroft has bought for \$8,100 the elevator of T. F. Keckler at Manley, Neb., and Nov. 1 will take possession.

Agent Omaha Elevator Co., Rising, Neb., Oct. 18: Wheat moving into market fast and new corn will soon be on the market; will be of fair quality; fall wheat doing fine.

A farmers' elevator company has been incorporated at Thayer, Neb., with \$10,000 capital stock. The company will not build an elevator but will handle shipments through a warehouse.

Christiansen & Cox and the Hampton Mill & Grain Co. have recently equipped their houses at Hampton, Neb., with gasoline engines. Mr. Greer is agent for the Hampton Mill & Grain Co.

C. E. Trump, agent for the Jaques Grain Co., at Ord, Neb., informs us that L. Spelts of David City is building an elevator four miles east of Ord on the B. & M. and will also build one at Ord.

Will Burns, recently of Sioux City, will superintend a line of elevators in Boyd county, Neb., on the new extension of the Fremont, Elkhorn & Missouri Valley road, for the Trans-Mississippi Grain Co.

The Conrad Grain Co. has been incorporated at Wood River, Neb., with \$5,000 capital stock and is building a 25,000-bushel crib elevator. Incorporators: Clancy St. Clair, John H. Bexton and William E. Shepard.

The elevator at Palisade, Neb., owned by the B. & M. R. R., and operated by F. C. Krotter & Co., caught fire from the engine, Sept. 15, and burned to the ground. The building contained about three loads of ear corn.

We are informed by J. H. Ritchie, agent for the Nye-Schneider-Fowler Co., that the farmers who were interested in the organization of a Farmers Elevator Co., at Albion, Neb., have decided to postpone further action in the matter until next year.

The Updike Grain Co. has recently completed 15,000-bushel elevators at Niobrara, Neb., and River Sioux, Ia., and is now building a plant of 15,000 bushels' capacity at Lynch, Neb. Houses of the same capacity will soon be begun at Anoka, Neb., and Bonesteel, S. D.

John G. Murray and brother James have purchased the elevator and coal bins of the Dunbar Elevator Co., at Dunbar, Neb., and will continue the business under the firm name of the Murray Grain Co. The other firm at Dunbar is the Duff Grain Co., with C. C. West as agent.

William P. Sargeant and Nora I. Duff were married Oct. 15 at Nebraska City, Neb. The groom is a leading business man of Colorado Springs, Colo., and the bride is the daughter of N. A. Duff, president of the Duff Grain Co. After a month in the East, they will return to Colorado Springs, where they will make their home.

The Atlas Elevator Co. has built an addition to its elevator at Dixon, Neb., and installed a combination scale and dump besides erecting an office and coal house. Mr. Carroll is agent. The Terwilliger & Dwight Grain Co. also is making repairs and intends to build next spring or as soon as the rush of the season is over this fall. P. M. Sullivan is agent for the latter company.

J. W. Holmquist has purchased the interest of J. G. Haines in the Haines-

Merriam-Meredith Co.'s elevator at Omaha, Neb., and the Merriam & Holmquist Co. has been incorporated at Omaha, with \$200,000 capital stock, to deal in grain and seeds, and operate this elevator and other elevators on the Milwaukee road owned by Mr. Holmquist. Incorporators: Nathan Merriam, James W. Holmquist and Olaf Holmquist.

Some time ago it was announced that W. N. Spellman had sold his elevator at Beatrice, Neb., to the Nebraska Elevator Co., and purchased one of the Local Grain Co. Neither of these deals were closed. Mr. Spellman will continue business at his old elevator and the Local Grain Co. has sold its elevators, both at Beatrice and Marysville, to F. H. Peavey & Co., and released them with C. T. Peavey as agent. It is believed that litigation will follow Mr. Spellman's reconsideration of the two trades.

NEBRASKA LETTER.

Fisher & Son at Sedan have been succeeded by Hull & Hertz.

Smallwood & Wall of Arcadia have sold their elevator to Adam Schaupp, Lincoln, Neb.

The Weare Comm. Co. of Chicago have opened a branch office in the Board of Trade Building, Omaha, Neb.

Very successful meetings of the Nebraska Grain Dealers Association have recently been held at Lincoln, Hebron, Auburn and Table Rock.

Samuelson & Nelson, who operate elevators at Mapps and Knox, Nebraska, with headquarters at Hiawatha, Kan., have purchased the J. W. Anderson elevator at Stromsburg, Neb., which was formerly operated by the Stromsburg Cereal Mills.

The Nebraska Grain Dealers Association has compiled a report of damage by frost to corn to Oct. 1, showing an injury of 25 per cent; between 1 and 2 per cent of which will be a total loss. Almost all of this injury will produce unmerchantable corn, but the greater bulk of it will still be serviceable for feeding. The North Platte country, as is usual, suffered the most. It is to be regretted that Nebraska's very promising prospects for an unusual crop year, should have had its wheat and oats first marred by excessive rain fall, and its corn later blighted by frost. Little if any No. 2 grain of any kind in the state this year.

Harry C. Miller, for years identified with the grain business at Omaha, having acted as broker for various commission companies throughout the East, and more recently for the Crescent Grain & Elev. Co. of St. Louis and Smith-Gamble of Baltimore, died unexpectedly Oct. 18. Mr. Miller was in his usual good health Saturday afternoon, and crossed to Council Bluffs to visit a married daughter. After getting off the street car, he experienced a pain in the region of his heart, and as he was suffering intense pain, a friend whom he met called a hack to take him to his destination; before reaching there Mr. Miller expired. It was afterwards discovered that his attack was neuralgia of the heart. Mr. Miller—Harry—as he was better known by his very large circle of friends, in addition to conducting a grain brokerage business with office in the Bee building, Omaha, was County Clerk of Douglas County, Nebraska, having been elected to that office about a year ago. Besides a wife and daughter, Mr. Miller leaves a host of friends who mourn his loss.—E. C.

NEW ENGLAND.

Harry Holstein, who until about a year ago, when he went into bankruptcy, conducted a grain and feed business at New Haven, Conn., has been charged with fraudulently concealing assets from creditors.

Some one pried open the door of the grain sheds belonging to C. H. Felker & Co., at Brockton, Mass., recently. The door fell against a chute, starting the grain, which flowed out on to the muddy ground, mingling with the mud and wasting a large quantity of grain.

S. V. Osborne has completed his elevator at Branford, Conn., and it is said to be one of the finest in the state. It is a five-story building of 20,000 bushels capacity, with stone foundation and cellar and is thoroughly equipped with elevating and cleaning machinery. A corn crusher, corn meal bolt, automatic separator and feed mill form part of his milling apparatus. The engine room, 18x24 feet, contains a 40-h.p. engine and a 50-h.p. boiler. The office is lighted by a bay window and the whole plant is wired for electric lights. Coal sheds are now underway and machinery for elevating and screening the coal will be installed. A witch hazel plant, a sawmill and hay sheds are the other buildings.

NEW JERSEY.

John W. Smith is having an elevator built in connection with his flour mill at Prallsville, N. J.

NEW YORK.

The F. H. Shulo Co. has been incorporated under the laws of New Jersey at New York City, with \$1,000,000 capital stock, to deal in grain.

Defaulters on contracts will find it hard to do business at Buffalo, N. Y., under the new rule of the Merchants Exchange prohibiting members from dealing with a defaulter who has been posted on the Exchange. Retail grain and feed dealers who fail to meet their contracts will have an opportunity to be heard before the committee.

T. H. Miner, New York: Late advices from Western correspondents are to the effect that Iowa, Illinois or Indiana corn is not in condition to stand shipping and that scarcity of cars promises to cause a further delay in the active movement of corn. Wheat seems to be ripe for a rise; the millers are now getting a profit of 50 cents a barrel on flour, and can stand it. The winter wheat acreage will be very large.

L. A. Morey, New York: Trade light, oats coming in hot, making losses heavy. The interior is feeding oats in preference to corn on account of difference in price. I have 50 cars of grain which have been detained on the P. R. R. since the middle of August. Our principal trouble with bleached oats is that the oats do not come in like sample. As a rule they are not bleached as much as sample. Much of the bleached oats get hot because they are not dried well before shipment. Recently out of a shipment of nine cars, eight arrived hot.

BUFFALO LETTER.

Complaint is made that the buckwheat crop is short. Dealers and millers are finding it hard to start up the usual fall business in that line. Other grains are more plentiful.

Grain receipts by lake are picking up,

being for the week ending Oct. 18 4,578,000 bushels, with 481,000 packages of flour, both considerably more than for some time. The grain is still mostly wheat from Lake Superior.

Receipts of oats are very light, in spite of the oceans of them that came in soon after harvest. It is thought that they were so poor in quality that they disgusted consumers, so that it was impossible to get even a fair price for them and now the producer is holding them back for an advance.

A grain man on 'Change is exhibiting a letter this week to show his increased admiration for the race. It is from a man to whom in 1867 he lent \$20. With the letter was a check for \$62, principal and interest for the 35 years the money had been out. He had entirely forgotten the transaction and cannot recall it by means of this reminder.

Canal men say that the battle is now won, as both parties have endorsed the policy of canal enlargement, accepting the 1,000-ton-barge size, as advocated by the Roosevelt commission. They will now go at work on the bill and get it ready for an early introduction this winter. It is too bad that politics killed the bill last winter, as it puts off the work another year.

Grain deals on 'Change that ended in a dispute seems to be numerous this year. The reference committee has been very active lately and has just posted two outside firms, forbidding members to deal with them till they settle. Some years this committee has no work to do; and there are never any disputes, as a rule, between Exchange members, that get as far as the committee.

A move is on foot to establish an official weighing bureau, for car grain after the plan of the inspection bureau and as the latter is giving such good satisfaction under Chief-Inspector Shanahan it is expected that his department will take care of the weighing also. This places the entire business under the Merchants' Exchange and cannot fail to add to the general tone of it.

The western practice of mixing wheat in with oats in the hope of selling the combination for more than they would bring apart, is complained of. Chief-Grain-Inspector Shanahan is down on all such practices and refuses to give such stuff any grade, as a rule. He will do what he can to drive the practice out, as it is far from straight business. Nobody here is accused of doing anything of the sort.

Buffalo is getting to be a center for cereal and corn milling and the clipping and transferring of car grain. It has long been a depot for damaged grain on account of the lake trade, but that industry is at a standstill now on account of the impossibility of getting coke or hard coal at the dryers. A cargo of canal grain was wet the other day and it was almost impossible to give it away, as nobody would take it except farmers in single wagon loads.

Sprouted wheat is the rule this year. An expert says that not less than 90 per cent of the state white that comes in here is grown and the white Michigan is fully as bad. The state white is the finest berry, but it seldom grades above three. Where it has been run through a machine and the long sprouts taken off there is often a sign of a sprout left. Millers are not afraid of it where they can sell the flour unbranded, but it is cheap stuff at the best.—J. C.

NORTH AND SOUTH DAKOTA

A. J. Clark, Minneapolis, Minn., Oct. 19: Crops near Vienna, S. D., good.

Martin Broton, agent for the Minneapolis & Northern Elevator Co., at Petersburg, N. D., has recently put in a gasoline engine.

The agent of the Union Elevator at Kempton, N. D., recently was caught in the belt and carried round the shafting to instant death.

The elevator of the Dakota Elevator Co. at Fessenden, N. D., was burned on the evening of Oct. 7, together with 20,000 bushels of grain and the coal sheds.

The timbers of the elevator of the Northwestern Elevator Co., at Bathgate, N. D., which was built in 1882, have become rotten and brittle, and on Oct. 6, the siding gave way, letting about 1,500 bushels of flax run out. It fell upon the roof of the driveway, crushing it and carrying it to the floor below. Repairs were begun immediately.

The J. T. Scroggs Elevator Co., of Carthage, S. D., receives grain from farmers for storage on the following terms: 1st: For receiving, handling, insuring, delivering and fifteen days storage, 2c per bushel. 2nd: If purchased by us, no charge will be made for first 15 days. 3rd: Storage charges after first 15 days, $\frac{1}{2}$ c per bushel for each 15 days or part thereof for first three months, after the first three months $\frac{1}{2}$ c per bushel for each 30 days or part thereof. 4th: If for any reason it shall become necessary to remove this grain, we reserve the right to deliver it from any other elevator or warehouse operated by us, subject to the same rate of freight to Duluth, St. Paul or Minneapolis as the tariff rate from this station. 5th: If this grain is cleansed at the request of the owner when delivered, a charge of $\frac{1}{2}$ c per bushel in addition to the above will be made. 6th: This grain is insured from fire for the benefit of the owner.

OHIO.

An elevator has been built at Savona, O.

J. H. Feldman has sold his elevator at St. Henry, O.

Clinton Sybarger will build an elevator at Sanville, O.

The only elevator at Ney, O., is owned by Raymond P. Lipe.

Sheets & Paul have succeeded Philip Sheets at Botkins, O.

J. T. Raudbaugh is out of the grain business at Celina, O.

W. N. Dingledine will enlarge his elevator at Buckland, O.

J. T. Robinson will build an elevator at Swanders Station, O.

B. D. Nepper & Bro. offer scoop shovel competition at Tiffin, O.

Buckley Bros. are now the only regular dealers at Kingman, O.

Brown Bros. have retired from the grain business at Newark.

Cuttingham Franklin has a 20,000-bushel elevator at Hamler.

The H. Daleat Baling Co. is no longer a dealer at Broughton, O.

O. R. Roszell is no longer in the grain business at New Carlisle, O.

Geo. Pierce conducts a scoop shovel business at New Carlisle, O.

Davis & Sterrett have succeeded S. D. Mead & Bro. at Marengo, O.

E. E. Evans & Co. are building a 8,000-bushel elevator at Kansas, O.

The Davis Mill Co. has succeeded the Shelby Mill Co. at Shelby, O.

The Vickery Grain Co. has succeeded Jones & Pearson at Vickery, O.

THE GRAIN DEALERS JOURNAL.

J. D. Jaques is employed at East Liberty, O., not a grain dealer.

B. Wagon ships grain by means of the scoop shovel at Stony Ridge, O.

W. Horn and A. E. Wetzel are scooper shippers at Unionville Center, O.

The Standard Cereal Co. is building an elevator and mill at Chillicothe, O.

The Gilboa Grain & Seed Co. is the successor of N. F. Dean at Gilboa, O.

The Burbank Elevator Co. is now the only regular dealer at Burbank, O.

G. R. Keller operates a flour mill at Arcanum, O., and buys some grain.

The A. H. May Co. is the successor of the Jay Grain Co. at New Bremen, O.

O. S. Marshall & Son are the only firm in the grain business at Hardin, O.

B. B. Yates has succeeded J. Heiskell & Son in the business at Woodlyn, O.

White & Barnard have discontinued their business in grain at Burbank, O.

J. F. Plice of Nankin, O., is attaching an auto-sparker to his gasoline engine.

Geo. Thierwechter of Oak Harbor owns the only elevator at Trowbridge, O.

The Myers-Patty Co. will put a corn dump in its warehouse at Maria Stein, O.

The Beuzman Flour Mills at Minster, O., buy grain regularly but do not ship.

S. A. Muff is making some improvements in his elevator at New Carlisle, O.

Harry Kress of Heno, O., is erecting an elevator with a corn sheller in connection.

H. H. Baer and J. B. Kindell are known as scoop shovelers at Covington, O.

Day & Williams and C. D. Resler are the present regular dealers at Caledonia, O.

Sheller & Disher of Decatur, Ind., are the only regular grain dealers at Elgin, O.

J. W. Simmons of Pemberton, O., is making some improvements on his elevator.

Ed. H. Wilson has purchased the elevator at Lippincott, O., of John B. Outram.

Several farmers of North Baltimore, O., ship their own and their neighbors' grain.

R. N. Walter has succeeded R. N. Walter & Co. in the grain business at Dundee, O.

The firm of J. J. Lloyd & Quale are no longer in the grain business at Grelton, O.

The regular dealers at Melvin, O., are Bennett, Thompson & Co. and M. C. Hoover.

Henderson & Coppock and Campbell & Everett are the regular dealers at West Milton, O.

Holcomb & Co. at Limestone, O., are hay buyers. Limestone has no grain warehouses.

Hall & Taggart, Plain City, O., are improving their elevator and putting in a chain drive.

Peck & Co., Chicago, O., Oct. 9:—Lots of wheat to be sown yet on account of wet weather.

The complete consolidation of all the local associations of Ohio seems to be near at hand.

John W. Browning has purchased the business of Miller & Lapham at New Philadelphia, O.

Gregg Bros., Grove City, O., Oct. 8: Wet weather is doing considerable damage to corn.

H. A. Myers has recently installed a No. 6 Clipper Cleaner in his elevator at Plankton, O.

Daniel Burns, Hagerman, O.: I take the Grain Dealers Journal and would not do without it.

J. M. Acton is expected to enter the scoop shovel business at Milledgeville, O., about Nov. 1.

Banian Bros. are building an elevator at South Solon, O., on the Detroit Southern road.

Frank Lee is not a regular grain dealer at LaRue, O., but is in the employ of W. F. Johnston.

J. B. Poyer and C. J. Sprankel scalp at Ceylon and Vermillion, O., when business is so they can.

Vanica & Jordan of East Liberty, O., do business under the name of the East Liberty Elevator Co.

The two elevators at Grove City, O., are owned and operated by Gregg Bros. and Johnson & Grant.

C. C. Walker is the only grain buyer at Hillsboro and he has only a small warehouse, no elevator.

Wm. Salen & Co., West Salem, O., Oct. 10: Wheat moving very slowly; farmers holding for more money.

W. S. Weston buys oats at New London, O., and ships them through the elevator of W. A. Day & Co.

W. S. Fogle will put in a gasoline engine and a French burr feed grinder in his house at Broadway, O.

Ferd Nukers, although not engaged in the grain business at Jewell, O., receives market reports and letters.

Dudley & Co., at one time known as the Wren Elevator Co., are the only regular dealers at Wren, O.

The Willshire Milling Co. is installing a No. 2½ Western Sheller and Cleaner in its elevator at Willshire, O.

Nutt, Allen & Co. will improve their elevator at St. Johns, O., and put in three more stands of elevators.

Clem Herrod is trying to do a scoop shovel business at Marengo, O., for Levering Bros., of Fredericktown, O.

Churchill & Co. now conduct their business at country stations under the name of the Standard Grain & Hay Co.

Frank Landis, who lives about seven miles away, has caused the grain dealers at LaRue, O., considerable trouble.

Franke Bros. have arranged their mill at Creston, O., for use as an elevator and deal in wheat, corn, oats and rye.

H. P. Sigler's elevator at Ritman, O., was burned on the morning of Oct. 12. Loss, \$6,000, with small insurance.

B. D. Knepper & Co., having headquarters at Tiffin, O., use the scoop at all the small sidings in Seneca county, O.

Ralph G. Spencer now owns and operates the elevator at Ottawa, O., formerly conducted by the Ottawa Grain Co.

J. G. Hickox & Co. are no longer in the grain business at Bowling Green, O., having sold out to F. E. Whitaker.

Allton & Smith are the successors of Allton Bros. at Swanders, O. Rhinehart Smith of Sidney is the new member.

Mr. Van Dickle and W. M. Reed of Bucyrus and Isaac Shearer of Monmouth, are among the list of Ohio scopers.

J. F. Fenton and C. S. Doolittle, sometimes under the firm name of Doolittle & Fenton, are scoop shovelers at Kent, O.

P. H. Daub will place two more elevator legs in his elevator at Millersville, O., where he is the only regular dealer.

John D. Owens has just completed a new 40,000-bushel elevator at Owen, O., to replace the house burned last spring.

McMorran Bros. are preparing their elevator at Thackery, O., and increasing the storage capacity about 5,000 bushels.

W. P. Adamson has purchased the elevator of the Pilliard Grain Co. at Roachton, O., and will continue the business.

Geo. H. Hay, whose plant at Creston,

O., was burned some time ago, is not now doing any business in grain at Creston.

S. J. Beeghly owns his own grain warehouse at Scipio Siding, O., but buys on commission for Comstock & Slessman of Clyde.

C. E. Young of Delphos, O., is doing a scoop shovel business at Weston at present. His regular business is in hay and straw.

P. J. Thompson, who has no facilities whatever, is trying to do a scoop shovel business at Plankton, O., and adjoining towns.

S. Slauson, manager, Ottawa, O., Oct. 10: Wheat good crop; corn extra good but damaged a little by continued wet weather.

Beedle & Makepeace have succeeded Beedle & Houser at Troy, O., and do business under the name of the Troy Tile & Brick Co.

Isaac Shearer of Monnette, O., and C. W. Mapes and B. E. Monnette of Tobias, are troubling the regular dealers in that vicinity.

M. F. Barrett will either rebuild his elevator and mill at Bainbridge, O., which were burned Aug. 28, or sell to those who will.

M. L. Hager now operates the elevator formerly owned by Hager & Hart at Versailles, O., but he is desirous of retiring from the business.

Ralph Hamilton of London, O., three miles east of Plattsburg, does some scoop shoveling, thus coming into competition with the Plattsburg dealer.

Tenney & Morgan, Newark, O., Oct. 10: Outlook for corn crop fair, although somewhat damaged by rain and frost; wheat seeding delayed by rains.

Nutt, Allen & Co., St. Johns, O., Oct. 10: Large crop of corn but much complaint of corn spoiling in the shock on account of the wet, warm weather.

M. S. Vought, R. J. Peters and H. C. Murphy are three farmers at Hooker, O., who try to ship their own grain. G. W. Lamb is the only regular dealer.

F. S. Hanley, Elmwood, O., Oct. 9: Had very little wheat; good run in oats; corn not in yet; much corn will not be marketable; too soft; frost caught it.

Harmonious meetings of regular dealers have been held this week at Washington C. H., and Circleville, O., with the result that local associations will be formed.

D. K. Gillespie has a 15,000-bushel elevator on the Miami & Erie Canal at Lockington, O., but he hauls his grain and loads it on the C. H. & D. tracks at Kirkwood.

S. A. Muff, New Carlisle, O., Oct. 10: Corn has been damaged to some extent by recent rains, but will still make a good crop; probably will be a great deal of soft corn.

O. F. Kimmel is putting a new sheller and corn cleaner of the Western make, one stand of elevators for corn and a chain drag belt in his plant at New Weston, O.

W. L. Bristol has recently completed a new engine room in connection with his elevator at Mt. Blanchard, O., and installed an ear corn elevator and a corn dump and drag.

Cyms, McGuffey & Son, McGuffey, O., Oct. 8: Corn crop in this vicinity unfit for market purposes and will nearly all be fed on the farms; will have very little corn for shipment.

Jacob Stall of Marshville and Geo. W. Myers of Canal Fulton, O., are buying grain from farmers. They have no facili-

ties for shipping it but order a car and load from wagons.

D. R. Risser has remodeled his elevator at Rimer, O., and installed a 60-h.p. boiler with three burrs for grinding. He also intends to build a 10,000-bushel elevator at Rushmore.

E. T. Woodcock writes that he is building a \$2,500 elevator and shelling station for corn and wheat at Dallas Switch, O., which he will operate in connection with his house at Urbana.

V. M. White, Climax, O., Oct. 9: Wheat good; large crop of oats but graded badly; some corn spoiling now on account of early frost and wet weather, but crop is large.

Spangler Bros., Defiance, O., Oct. 9: Corn injured some by wet weather; very little seeding done; acreage will not be one-half what it would have been on account of wet weather.

The W. J. Jenkins Grain Co. has improved its elevator at St. Paris, O., and put in an Eureka Clipper, a Boss Grain Loader and an Ohio Seed Cleaner. It is also building a feed house.

J. E. Wells & Co., Quincy, O., Oct. 11: Corn crop will be large but of poor quality as frost came before all was ripe. It is so wet that only about one-half of the wheat acreage will be sown.

J. A. Bill of Wharton, O., thinks it ties up too much money to build or buy an elevator and so he loads grain direct from the wagon to cars. W. L. Bristol owns the only elevator at Wharton.

J. W. Murphy, manager, Octa Grain Co., Octa, O., Oct. 11: Farmers have begun to crib some corn but none ready for market yet; wheat seeding well along, the majority being fertilized.

Gordon & Dehring, Curtice, O., Oct. 12: Corn crop not damaged any by rainy weather; average yield much better than last year; received a load of new corn Oct. 10, which is almost fit to shell.

Chas. T. Pierce, Defiance, O., Oct. 9: Very little seeding done here yet on account of about two weeks' successive rains; corn has been damaged some, but not as much as some try to make out.

J. L. Barnes & Son, Milledgeville, O., Oct. 15: But little corn being cribbed as yet, too sappy; won't do to ship before Nov. 1 to Nov. 10; promises to be fair yield and quality and to move freely.

Coppess & Dohme, Stelvideo, O., Oct. 11: Will have large crop of corn to handle; yield about 78 bushels to the acre; will not be fit to handle before Nov. 1; wheat in the ground looking fine.

W. H. Potter, East Liberty, O., Oct. 8: Wet weather has delayed seeding and injured corn in the shock; about the usual acreage of wheat will be sown; pastures good; grain not moving to market very freely.

E. M. Montgomery, Newark, O.: From one of the largest this place has come to be one of the smallest shippers of grain in the state, partly on account of the small stations outside where the scoop prevails.

J. R. Raudebaugh, Greenville, O., Oct. 9: Almost as quiet in the grain business just now as a church yard at midnight; farmers report that quite a large per cent of the new corn will be injured by the wet weather.

The Miami Valley Grain Dealers Association held a banquet at Hotel Steinburger, Wapakoneta, O., Tuesday night, Oct. 21. This organization is growing and doing much good work for the dealers of that city.

Elliott & Armstrong have just comple-

ed a mill outfit in their warehouse at Millersburg, O. They have also made improvements and installed a 25-h.p. steam plant, three stands of elevators and attrition mills.

H. E. Rhoades, president of the Tiffin Grain Co., writes that the company has recently completed a 30,000-bushel elevator at Tiffin. Wilmer Kipe & Co. are connected with the grain department of this company.

W. L. Bristol, Mt. Blanchard, O., Oct. 13: Farmers have only finished about one-third or one-fourth of their seeding and are still turning their clover seed; the rain last night will put them back still further.

The Bakhaus & Kuenzel Co. is enlarging and improving its old elevator at New Bremen, O.; and machinery, including a corn sheller and cleaner and a warehouse cleaner for wheat and oats has been ordered.

Falknor & Klepinger of West Milton, O., have purchased the elevator at Kessler, O., formerly owned by Victor Campbell, deceased. They are remodeling the entire plant and putting in the latest improved machinery.

The elevator formerly owned by Axel White at Hedges, O., has been moved to Goodwin, where in connection with those at Broughton and Melrose, it is operated by the Broughton Elevator Co., with Axel White as manager.

F. S. Hanley, while he admits that his elevator at Elmwood, O., is small, thinks it a very good one. His machinery includes two stands of elevators, a Pease blower and sheller, a corn sheller, corn cleaner and a corn loader.

Swetland & Crowell, Fredericktown, O., Oct. 14: Oat crop good, but damaged by rain; corn crop fair, but damaged by frost and rain; wheat crop good; hay about one-half crop, compared with last year; fall pasture fine.

Gordon & Dehring are the successors of Gordon & Heinze at Curtice, O. They will put in a 500-bushel No. 2½ Western Shaker Corn Cleaner and Sheller with a view of using the cleaner as a receiving cleaner. H. G. Dehring was formerly at Oak Harbor.

Baldwin & Elliott, West Liberty, O., Oct. 11: Weather has been extremely wet; about one-half of the wheat not sowed; very bad fall to handle corn; some molding in the shock; hardly think any in this vicinity will grade this year; lots of soft corn.

E. E. Evans of Tiffin, O., is not a regular dealer. He buys some grain at other stations on the scoop shovel plan, but not at Tiffin, as the grain dealers might go into the hay business. The regular dealers are Jos. Loudenslager, Jr., and Sneath & Cunningham.

The delay of grain in the railroad yards at Columbus has become chronic and many cars of grain have been detained 16 to 22 days. This has placed such a check on the business of Columbus dealers that all joined in a formal protest and petition for relief to the railroad companies last week.

G. M. Noggle, Weavers Station, O., Oct. 10: We have a fair crop of corn if the rain ceases; have had very bad spell which has damaged the corn soon; can not yet estimate damage; average crop of wheat and of good quality; no rain in harvest; oats extra good; seeding about all done.

C. B. Jenkins, president of the Marion Milling & Grain Co., Marion, O., is the publisher of a semimonthly farmers' bulletin which has nearly 1,000 subscribers among the farmers in his territory. The

publication was begun 3 years ago, and has proved so popular that it will be continued indefinitely.

Although no scoop shovel men live at Nankin, O., some from a distance have caused dealers some trouble. W. C. Haines of Spencer was one of these. Several years ago he got one car of oats. Since then he has gone into bankruptcy, beating a number of farmers out of the hay he had bought.

J. F. Plice, Nankin, O., Oct. 11: Early sown wheat looking well but some is just being sowed, having been delayed by the long spell of rains; ground hardly fit yet to cover grain that is being drilled; quality of wheat and oats injured by rain although yield is good; second crop of hay quite large.

The fire which destroyed the elevator of J. W. McMillen & Co., at Grover Hill, O., Oct. 2, is supposed to have originated in the cob room. Loss on grain and elevator \$8,200, with \$12,000 insurance. The loss was promptly adjusted and paid, and preparations are being made for the rebuilding of the plant.

Zebulon H. Travis, who owns 21 elevators in Ohio and Indiana, and makes his headquarters at Toledo, is a lover of horses. His judgment in buying a horse was vindicated recently when the gray gelding, Dr. Strong, trained under his direction, won the 2:17 trot at Lexington, Ky., Oct. 7, against 24 starters.

A very harmonious joint meeting of the Grain Dealers and Millers Association of Northern Ohio, the Grain Dealers Association of North-Western Ohio and the North-Western Ohio and the North-Eastern Indiana Associations took place in the Produce Exchange at Toledo, O. Oct. 8. Over 100 were present and a very successful meeting was held.

Wilmer Kipe & Co., Tiffin, O., write: We buy from elevators only. Never buy from farmers here, as we have all we can do without. H. C. Rhoades and Wallis Trumpler compose the Tiffin Grain Co. They have turned the old mill into an elevator, for which power is furnished by a 15-h.p. electric motor, and are buying grain. No new elevators here except Sneath & Cunningham's and the Tiffin Grain Co.'s. There are some firms who do a scoop shovel business through some small fellow in the country. Some small shippers have store boxes at several places and call them elevators.

Grayson Dye, who claims to be a dealer in grain and flour, recently bought a car of oats at Conover, O., and shipped it to Pittsburgh, Pa. This is his first shipment in that part of Ohio, but he has promised the farmers in the vicinity of Fletcher, Conover and Troy that he will be prepared to load at any of these stations.

John M. Cronenberg, a man well known to the grain trade, died at his home in Toledo, O., Oct. 5, aged 46 years. For five years Mr. Cronenberg was manager of the Western Union on the Toledo Produce Exchange. Later in connection with Ed King, under the firm name of Cronenberg & King, he conducted a commission business. At one time he represented Schwartz, Dupee & Co. of Chicago and of late has had charge of the Toledo business of Ware & Leland. A wife, two daughters and a son survive him.

The Cincinnati Chamber of Commerce has adopted the following new rules for the grading of oats: No. 2 white shall be not less than seven-eighths white, sound and reasonably clean. No. 3 white shall be not less than seven-eighths white, sound, slightly stained, or mixed with other grain, and reasonably clean. No. 4 white shall include all sound oats, about seven-

eighths white, stained, light weight, badly mixed with other grain, or from any other cause unfit for No. 3. No. 3 mixed shall be sound, too light, too much stained or mixed with other grain for No. 2. No. 4 mixed shall include all sound oats, light weight, badly stained, badly mixed with other grain, or from any other cause unfit for No. 3. Rejected shall include all unsound oats, badly mixed with other grain, dirty, or from any other cause unfit for No. 4.

The committee appointed at the recent meeting of the state association, consisting of H. S. Grimes, E. A. Grubbs and C. H. Tingley, to work with the Central Freight Association in hope of securing some action which will insure the prompter delivery of new corn, is already hard at work with the chairman of the association, J. F. Tucker, Chicago. The committee has asked that the railroads issue large cards bearing the following in large type: "THIS CAR CONTAINS NEW CORN; RUSH." It is desired that these cards be tacked to both sides of each and every car of new corn and that all yard-masters and other interested employes in the transportation department be instructed to give cars so tagged the preference when making up east-bound trains. The move is one which merits the hearty co-operation of every corn shipper.

OHIO LETTER.

J. M. Deweese of Montezuma has purchased the elevator of J. W. Macy at Yorkshire, O., and Nov. 20 will take possession.

The basket factory of J. S. Fetten of Edgerton was destroyed by fire recently. This was one of the many branches of business which with his grain and store make him a busy man.

All dealers at the Van Wert meeting are a unit that the corn is in bad shape in the shock. Wheat seeding very backward and only the very best of weather will allow further seeding.

On the night of Oct. 11 the Weidman Flour Mills at Paulding were burned. Frank L. Windiate purchased the plant about a year ago and made improvements costing about \$4,000, in the way of new power and machinery. Loss about \$15,000; insurance on mill \$12,000, machinery \$5,500, brick barn \$1,000, office furniture \$500, grain and warehouse \$1,000, making a total of \$25,000. Windiate is undecided as to rebuilding. It is a good location for a mill.

The Northwestern Ohio & Eastern Indiana Grain Dealers Association met Oct. 16 at Van Wert, O., in Longwell Bros.' office and held a very interesting meeting. Corn, barley, new members and expenses were up for discussion. The item of expense was thoroughly discussed and finance committee of the following members was appointed: Tinkham, Carroll, Niezer, Egly, McMillen, T. Ireton, to report Oct. 23. The traveling man was laid off for a week. After the committee meeting a final decision will be made. The meeting adjourned to meet in Fort Wayne Oct. 23. The following members were present: From Ohio—Wm. Longwell, Frank Longwell, Dr. Gilliland, J. W. McMillen, Tom Ireton and Ben Ireton, Van Wert; J. L. Pocock and C. A. Bissell, Antwerp; Doc Dull, Celina; Arthur Dull, Rockford; John Haviland, Haviland; H. C. Tinkham, Latty; J. M. Deweese, Montezuma; H. G. Pollock, Middlepoint; D. W. Long, Convoy; from Indiana: Secretary C. G. Egly, Berne; President E. L. Carroll and H. G. Sheller, De-

catur. James Fagan, representative of R. Tuttle, Columbia City, Ind., came as a visitor.—C. A. B.

PACIFIC COAST.

D. E. Lyon has purchased the grain business of H. Eberting & Son at Hoquiam, Wash.

F. Keiser & Co. have purchased the wholesale grain business of Wm. Nelson & Co. at Spokane, Wash.

Richelieu E. Wheelan, San Francisco, Cal.: I appreciate your paper very much as being a valuable aid to those wishing to keep posted as to events in the grain world.

The wheat crop of Washington is said to have been marketed more rapidly this season than ever before. Millers and exporters have been competing closely for blue stem; and the prospects are that good wheat will be in short supply before the end of the crop year.

On account of the drouth and failure of crops in Australia the demand for wheat to go to South Africa has been diverted to the Pacific Coast. So far this season the exports of wheat and flour from Portland to South Africa have been over twice those of any preceding season. The total shipments of wheat and of flour as wheat from all ports in Oregon and Washington during the first nine months of this year are 2,937,000 bushels to South Africa.

The Northern Pacific Ry. Co. has found that the storage capacity of its warehouse at Seattle, Wash., is insufficient and plans are being made for the erection of a warehouse at West Seattle. To provide for immediate storage the second story of the warehouse now in use will be repaired and new approaches and incline be built. This lack of storage capacity is due to the fact that prices and railroad rates are low and buyers are sending their grain directly to the coast, there to await further shipment.

W. S. Sibson, Portland, Ore.: Our wheat crop in Oregon, Washington and Idaho is fully 30 per cent short of last year; instead of 50,000,000 we have not over 35,000,000, with less seed and consumption. Oriental and African flour trade leaves only about half as much wheat available for United Kingdom shipment as there was last year. Africa has bought here largely of wheat and flour in last ninety days and the oriental flour demand is large, many mills being contracted up to their capacity to the end of the year. Millers are keen buyers and as high as 72 cents per bushel has been paid for blue-stem wheat within a day or two. This is Chicago price.

PENNSYLVANIA.

D. G. Brandt and William Burges of Jonestown, Pa., have leased a large mill and grain depot at Boiling Spring, Pa., and will soon change locations.

D. W. Foust & Son have started in the grain business at Chambersburg, Pa., and will operate the elevator on the C. V. R. R. formerly owned by Mehaffy & Berlin.

Responsible Philadelphia receivers inform us that over 10,000 loaded cars are delayed on the Pennsylvania lines between Alleghany and Philadelphia, not because cars are scarce, but because of a shortage of locomotives.

E. E. Delp, Philadelphia: We will soon be handling lots of new dry corn from Delaware and eastern Maryland. The price f. o. b. Philadelphia ranges about 50 cents per bushel of 70 pounds of ear

corn. This eastern corn will supply the demand for cheaper corn and relieve the market until western corn begins to move.

Harry M. Edenborn, Philadelphia: The trade is buying most of its supplies at present from interior Indiana and Ohio shippers. As Chicago has no cut rate the interior shippers are able to undersell Chicago about one-half a cent. The trade prefers country oats at this time, but later all will have to go to Chicago for supplies. Demand for oats of good grade is fair and will increase as soon as cold weather comes.

The Paxton Flour & Feed Co. has completed an 18,000-bushel elevator at Bridgeport, Pa., which will be equipped with an Invincible Cleaner, a Cyclone Dust Collector, two legs and one screw conveyor, and run by electric power. Nelson Williams is local manager. The company also has a 70,000-bushel elevator and office at Harrisburg; a 15,000-bushel plant at Kerrsville in charge of Theo. Goodwin; a 15,000-bushel elevator at Bowmansdale, A. L. Landis, manager; one of 16,000 bushels capacity at Huntsdale, S. H. Carrothers, manager; one at Carlisle of 20,000 bushels capacity, Levi Hartzler, manager; at Middlesex a 14,000-bushel plant in charge of H. A. Voglesong, and at Trindle Spring a 12,000-bushel elevator with J. N. Strock as manager. Levi Brandt is general superintendent.

L. J. Logan & Co., Philadelphia: We find it very difficult to do any business. Cars are delayed so long in transit. Oats get hot and new corn will get even hotter if shippers attempt to ship it this year unless the railroad companies are able to make prompter deliveries. We had one car arrive at Harris, R. I., Oct. 18, which was started from Decatur, Ill., Aug. 13, and yet the eastern line over which it had passed denied receipt of car from western connection. So many cars are out a month that we are almost discouraged attempting to do any business. We believe the railroad companies should be held responsible for losses due to grain getting out of condition where delayed. The delay on the Pennsylvania lines has been worse than on other lines because its rolling stock has been fully engaged transporting soft coal.

SOUTHEAST.

W. C. Agee & Co. of Birmingham, Ala., have moved into their new warehouse, which is well arranged for the business.

J. S. Gray, grain commission merchant, has leased a warehouse on the Seaboard Air Line at Raleigh, N. C., and will deal in grain and groceries.

The White Hall Grain, Building & Supply Co. has been incorporated at Towson, Md., with \$15,000 capital stock. Incorporators: Nelson Gilbert, Christopher Slade and others.

J. C. Curry, Bessemer, Ala., Oct. 20: Many complaints of oats being stained and musty. Grain crop of Alabama as a whole is a failure. In the central and southern parts of the state it is a complete failure.

William F. Wheatley, secretary of the Baltimore Chamber of Commerce, and Miss Teresa Millholland, daughter of Dr. and Mrs. Millholland of Baltimore, will be married Oct. 25. Mr. Wheatley is well and favorably known among those connected with the trade at Baltimore and holds the good will of all.

The first new white corn received at Baltimore, Md., reached there Oct. 15, having been sent from Caroline County, Md., and consigned to J. K. B. Emory & Co. It showed fair condition and sold for

65 cents a bushel. Last year a lot of this corn reached Baltimore Sept. 27 from Virginia. It was in a dry condition and sold for 63 cents per bushel.

SOUTHWEST.

W. W. Baucum has entered the grain business at Leger, Okla.

Phillips & Olson have purchased the grain and coal business of T. J. Flynn at Aspen, Colo.

Seldomridge Bros. of Colorado Springs intend to build an 18,000-bushel elevator, costing about \$10,000, at Denver, Colo.

J. E. Robinson, chief inspector of the New Orleans Maritime & Merchants' Exchange, Ltd., reports that during the month of September 2,487,178 bushels of wheat were exported from New Orleans, compared with 3,051,093 bushels exported during the corresponding month of last year.

TEXAS.

C. Brown has purchased the grain business of J. M. Sarchy at Gail, Tex.

E. E. Roahen of Claflin has taken charge of an elevator at San Antonio, Tex.

The Weatherford Milling Co. has been incorporated at Weatherford, Tex., and will erect an elevator.

Farmers in Travis county, Texas, will need to ship in 300,000 bushels of corn, and contemplate arranging to buy direct from producers in Kansas and the Indian Territory.

The Werkheiser-Polk Mill & Elevator Co. of Temple, Tex., at a recent meeting of the directors, declared a dividend of 10 per cent. A. J. Jarrell has succeeded Chas. M. Campbell as director.

The executive committee of the Rice Association of America held a meeting at Houston, Tex., Oct. 17, and voted to promote the use of rice as food by employing a demonstrator in a rice kitchen at Chicago. The date for the next annual meeting of the national association was set the third Tuesday in January and the place Houston, Tex.

Edward Courtney, who has charge of the macaroni demonstration farms which the St. Louis and San Francisco Railway is establishing near Brady, Brownwood and Comanche, Tex., on Oct. 10 visited Governor Sayers and Commissioner of Agriculture Johnson at Austin, Tex., to explain the possibilities of the macaroni wheat industry in the semi-arid regions of this country. He is anxious that the state should establish an agricultural experimental station at some point on the line of the Fort Worth and Rio Grande Railroad, and the subject was discussed with Governor Sayers and Commissioner Johnson, who favor such provision being made by the next legislature. It is thought that the legislature at its forthcoming session will make provision for establishing two additional experimental stations, one to be located in the black land country and the other in the semi-arid region of West Texas.

TEXAS LETTER.

The matter of grading oats from Missouri is still fermenting, but likely to be settled at any time.

The weather conditions have been just right for the past month for the planting of small grains and the area that has been given over to this crop is much larger than ever before and is materially increased over that of last year. In many sections of the state the wheat and oats have come up and are making a fine stand,

and, with a good spring season, the crop will be enormous.

At McKinney, which is the center of the North Texas grain district, on Oct. 6 the Grain Exchange of that city met and the following officers were elected for the ensuing term: H. H. Crouch, president; R. D. Kitching, vice president; Edwin B. Doggett, secretary. This exchange will hold a daily meeting in the future. At the meeting a strong resolution was passed in regard to the manner in which the railroads have been handling the grain. Cars which, if handled expeditiously, would reach their destination in six days frequently do not arrive for a month or six weeks. In such cases the grain men are the losers, as their customers quite frequently cancel the orders on account of non-delivery or other reasons growing out of this cause.

The Texas Millers Association held a meeting at Ft. Worth Oct. 16 to discuss the Kansas milling in transit matter. They were advised that the Interstate Commerce Commission had made a ruling lowering the differential between flour and wheat from 5 cents per 100 pounds to 2½ cents per 100 pounds. The Kansas millers had asked that this differential be removed altogether and make the same rate on flour and wheat into Texas. The Texas millers were bitterly opposed to any reduction in the differential, and the result is by no means satisfactory to the Texas mills. The mills in Texas this year on account of the shortage in the wheat crop are forced to go north after grain to keep their mills running and if the Kansas mills can ship flour at the same rate as wheat it means ruin for the Texas millers for this season. The Texas millers will make a very strong effort to have this ruling abolished by the Interstate Commerce Commission. J. J. Johnson of the Galveston Freight Bureau was in attendance at the meeting. After the meeting the citizens of Fort Worth tendered the millers a spread and a drive over the city.—J. S. W.

WISCONSIN.

C. E. Berg contemplates erecting an elevator at Rio, Wis., early next spring.

N. W. Hoag will continue the business formerly conducted by himself and Henry S. Hoag at Delavan, Wis.

Rumor has it that Wisconsin and North Dakota will combine to establish state grain inspection at West Superior, Wis.

Grain dealers at Superior, Wis., allege that the railroads discriminate in favor of Minneapolis in rates on grain from North and South Dakota, and are urging the Duluth grain men to present the matter to the Minnesota Railroad and Warehouse Commissioners.

After having been closed for several months on account of the high price of flax, the flax mill at Superior, Wis., has resumed operations. John Bannon, the proprietor, has recently returned from a trip through the flax district inspecting crop conditions.

E. B. Hadden of the Hadden-Rodec Co., grain commission merchants, has been suspended from the Milwaukee Chamber of Commerce for three months for having made a cross trade with W. W. Bennett, Jr., who was expelled for alleged bucket-shopping.

The P. D. Murphy Grain & Mercantile Co. has completed its elevator at Welcome, Wis., which occupies a ground space 37x47 feet and is 40 feet high. The milling machinery includes a 9x24 three pair high feed roll, a Bowsher Feed Mill, a Reliance Scalper and a corn and cob

crusher. Power is furnished by a steam engine.

The elevator plant which Lieg & Dillenburg are building at Shawano, Wis., will be 48x60 feet, besides the office building, and have grain bins of 12,000 bushels capacity and warehouse capacity for 3,000 bushels of potatoes. The elevator proper will be completely equipped with weighing and elevator machinery, and power is furnished by a gasoline engine.

Fire, which is supposed to have originated from overheated shafting in the storage and conveying department, destroyed the plant of the Burlington Malting Co. at Burlington, Wis., Oct. 8, together with about 50,000 bushels of barley and malt and the machinery. A. Schwill & Co. of Chicago were the lessees and operators of the plant and the owners of its contents. Total loss, \$92,000; insurance, \$75,000.

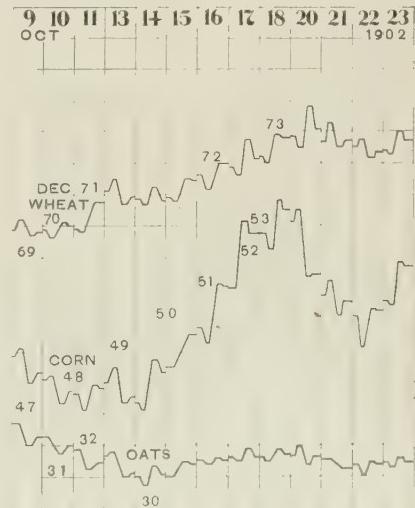
The Carpenter-Cook Co. has purchased property at Marinette, Wis., including the site of the Ludington, Wells & Van Schaick Mill. It is the opinion of C. I. Cook, one of the company, that a railroad built westward to Minneapolis and St. Paul is the only thing needed to turn Marinette into an important shipping point and that before many years the river front will be lined with elevators and grist mills. When the time comes the company will build elevators.

Under the new rule adopted Oct. 9 by the Milwaukee Chamber of Commerce no fear need be entertained by buyers of options at the Cream City that grain inspected at Milwaukee will be tendered. Besides the regular Chicago certificates only No. 1 hard wheat and No. 1 yellow corn can be delivered. The effect is to lift the Milwaukee market to a parity with Chicago and to keep it there. On the day following the adoption of the new rule transactions in the option market increased over 100 per cent.

Imports of rice during the eight months ending Sept. 1 amounted to 58,969,693 pounds, as reported by O. P. Austin, chief of the bureau of statistics; compared with 59,748,954 pounds imported during the corresponding months of 1901.

Prices at Chicago.

The opening, high, low and closing quotations on wheat, corn and oats for the December delivery at Chicago for two weeks prior to Oct. 24, are given on the chart herewith:



Hearing on Grain Grading at Chicago.

The Illinois Railroad and Warehouse Commissioners gave a hearing in their offices, Chicago, Oct. 22, to all who had any complaints to make regarding grading of grain at Chicago. The meeting was well attended by Chicago grain men, and was presided over by Commissioners French and Neville. Chief Inspector Bidwill and Supervising Inspector Smillie were also present.

Mr. H. B. Slaughter, Chairman of the Warehouse Committee of the Board of Trade, said: A great number of complaints brought to the Inspection Committee of the Board of Trade are in regard to irregularity in grade. The Railroad and Warehouse Commissioners have set a time to-day to hear all complaints and the Board of Trade had a notice posted on the blackboard that all might participate in the meeting. This is the time for any who have complaints about the inspection department to make them known. The Railroad and Warehouse Commission are ready to hear any complaint and promises to do whatever is in its power to make grading here more uniform and in keeping with the ideas of the trade so far as it can. It is in its power to lower the grades. If you have any suggestions do not be backward about making them known.

President W. S. Warren of the Board of Trade spoke as follows: As the executive head of the Chicago Board of Trade, I would like to say that we appreciate this opportunity you have given us. We think there should be a community of interest between your honorable board and the members of the Board of Trade, representing as we do shippers of grain from the country and being ourselves large owners of the grain upon which you have to pass. I wish there might have been a greater representation of our membership here than there is.

While on my feet I would like to say something regarding my personal views on inspection in general. I notice in the call for this meeting, one of the objects of the meeting was to place our inspection more on an equality with other markets, and I have heard a great deal of talk of one kind and another (I do not know where it emanates from) regarding lowering our grades. I would like to say that personally I am opposed to any lowering of our grades. I believe that Chicago owes her supremacy as a grain market as much to the fact that she has maintained her grades as to any other one fact. I think the history of the various commercial exchanges of the country will show that those who have had a vacillating system, coming and going with the different crops, or who have had a low grade of standard grain have gradually wasted away and lost their positions. It seems to me just at this time that the subject uppermost in the minds of the trade is the question of grades, and we hear from our country friends and from various people suggestions of lowering the grade of corn; Toledo has done this and Baltimore has done that and St. Louis has done something else and let us get into line with them. I do not think that anything could be done more inimical to this market or to the Chicago Board of Trade than to lower our grade of corn. We have here the greatest speculative market. We have in fact all future trade in corn there is in this country. We have the lion's share of the actual corn. Under our inspection rules

as they have been for years we have built up this market and we will make the mistake of our lives if we go to putting down our grades.

There are many here who can discuss the question of grading from a technical standpoint much more knowingly and intelligently than I can, but I think in general terms, I have expressed the sentiment of the majority of the members of the Board of Trade.

Mr. Warren was asked to express his opinion on the oat situation and said: I think the grade of standard oats has been made too low. Our oats market, from a gradual and constant growth, of the past two or three years until it reached the maximum in the volume of business, has suddenly dropped to about the minimum of any time for a number of years. From my observation and from the observation of others, which has been reported to me, I believe that it is due largely to the fact that the standard of our contract grade of oats has been placed too low.

R. Gambrill stated that the grade was too high and that it should be made more in conformity with the grades of other markets.

Mr. Neville said to Mr. Gambrill: I want to ask as a favor to the commission, that you kindly draft a rule that you think would be proper and submit it to us for our assistance. We would be very glad to have you do that. I would like to ask you to draft, to-day or to-morrow, a rule which you think would cover the matter better than the present rule.

Mr. Gambrill: We think the present rule would cover everything provided they do not go above that. Make the out inspection equal to the in inspection and I do not think there will be any trouble in regard to grades, and I do not think there will be any complaint whatever.

H. Hemmelgarn: This in inspection and out inspection—I would like to have it explained if there are two inspections, if there are two different grades. Do we understand we have to deliver higher grades than we get out? Now such a thing ought not to be passed over lightly but ought to be investigated. This inspection is a very serious matter and before they change it we ought to know what we are doing and what we have been doing.

N. W. Bartlett: I think the most valuable asset of the Chicago Board of Trade is the confidence of the public in the stability of its grades. Do anything to shake that confidence, lead the public to believe that some temporary condition or personal advantage will lead us to a change in our standard grade and I think we will deal a blow to our trade here, both in cash and speculative that we will never get over. As President Warren has stated the history of such a course can be traced to other western markets. Milwaukee went in for that sort of thing and its cash business left it. Kansas City grades have always been under suspicion and one of the redeeming features of this market has been the fact that the public has reason to have confidence in the stability of our grades. Mr. Gambrill speaks of Baltimore grades and thinks our grade should be on the same basis, while the two markets are situated so differently, Baltimore not being a carrier of grain, the grain simply passing through there in transit. Chicago buys millions of bushels of grain every year to carry through the germinating season and there is every reason why there should be a difference between Chicago and Baltimore grades of corn. If it is given out

to the public that there is to be any change in our grade of corn, even if that change is to affect old corn only, you will shake public confidence in our contract grade. The public naturally will think there is only a remnant of the old crop left, and they will take it for granted that there is going to be some letting down on our inspection of the new corn. The public won't come in and buy corn if there is to be any doubt whatever as to the corn that is to be delivered to them in May. The argument used when we changed to 2 mixed oats instead of standard oats was that 90 per cent of the oats raised in this country was white oats. The intention was to take in the better class of 3 oats. If it were the intention to make a commercial grade we seem to have failed. Only 80,000 bushels went out of this market of standard grade during the past month. The same argument applies to oats as to corn. If the buyer of a May option has any doubt in regard to the commercial character of corn that is to be delivered to him at this market he is not going to be a buyer in this market. As President Warren has stated up to the change of standard grade of oats we had a large speculative business in oats. At present this is very light. It is my theory that it is uncertainty relative to the grade of standard oats that has driven this speculation away. I think that at the present time the oats that are being inspected as standard, so far as I can find out are about of equal quality to those that were taken in, but there was a time when oats taken in as standard grade should not have been taken in on account of having too many unsound oats and these are the oats that at the present time are a drag on the market.

W. N. Eckhardt said: Regarding inspection, I do not know that I differ much from what President Warren has said. I think the corn should be dry and grade No. 2, though I feel that the department is a little too stringent about damaged corn and dirt in the corn. The quality seems too high. It ought to carry a reasonable amount of damaged corn and a little dirt. I think the inspection should be more uniform than we have at the present time. I am satisfied that the inspection could be made more uniform if these inspectors had some idea of how to form standard or other grades, or had some standard to go by. The inspection is so irregular that interior trade with country shippers has been discouraged. They say they can't ship here and get any decent grade. I think oats are too low for standard oats. I do not think it was the intention they should be made good enough to make oat meal out of, but I think that it should be graded on a basis that would be more satisfactory to trade in general and insure more satisfaction to the trade, and the trade would have more confidence in it.

H. Hemmelgarn: There is a rule on the Board of Trade that white oats shall be $\frac{1}{8}$ white, and $\frac{1}{8}$ black oats is admissible. It said nothing about light mixed oats. Anything might be included in the $\frac{1}{8}$. If the inspectors are here it ought to be stated what the $\frac{1}{8}$ is to be. If this $\frac{1}{8}$ is to include anything, then there is no wonder that they take such stuff in the elevators to-day that no one wants to buy it. There are people that say "If this is what I get on standard oats, I won't buy another bushel of oats." If it means that it is to be $\frac{1}{8}$ white, and $\frac{1}{8}$ you do not want, then I hope you will soon say what it shall be.

Mr. French: I want to say that when

this rule was presented and asked almost unanimously to be adopted, we called attention to that very thing and objected to it as strenuously as we consistently could and whenever you gentlemen see fit to have it changed we will be glad to change it, if it is the proper thing to do.

Geo. A. Stibbens, Secy. Grain Dealers National Association, said: I have in my office samples of grain prepared by the Chicago Inspection Department, representing the different grades of grain according to rules. The samples of yellow corn consist of $\frac{3}{4}$ yellow and $\frac{1}{4}$ white; white corn, $\frac{7}{8}$ white and $\frac{1}{8}$ yellow; white oats, $\frac{7}{8}$ white and $\frac{1}{8}$ black. I have called the attention of Mr. Bidwill to the fact that he never graded a car of corn according to these rules. I contend that should I ship corn to Chicago during a corner, mixed in the same proportions as the samples representing 2 yellow or 2 white corn, and on account of that mixture the inspection department would fail to grade that corn contract, I could bring suit against the inspection department and recover the damage. This is my personal view of the matter and I think it would be well for this department to prepare samples of corn that would represent the grade it was intended to represent.

Mr. Bidwill: I wish to state that I believe every gentleman here has seen these samples. We fixed up samples of $\frac{7}{8}$ white oats and $\frac{1}{8}$ black and there isn't a man here that would accept them as No. 2 white oats. I have recommended to the commission that a change should be made and there is no better time than now to discuss that matter. I believe this grade should be changed in conformity with what Mr. Hemmelgarn has talked about. When the corner was on, I was asked by a shipper if he shipped 7 cars of white oats and 1 car of black, if that would grade No. 2 white. I don't think that rule ought to be left there. When these samples were made up I thought best to show them to Mr. Stibbens as secretary of the Grain Dealers National Association. What is the use of having inspection rules when your men can't live up to them and if we made the inspection according to our rules, there isn't a man among you that wouldn't kick. If you take seven small glasses of white oats and mix them with one glass of black oats not one of you would call them No. 2 white oats.

C. Counselman: I think in the first place we are talking at random here. This meeting was called for changing the inspection of No. 2 corn. I am distinctly not favorable to letting down the inspection on No. 2 corn, coming out of the new crop, what we call winter shell corn. I believe everybody thinks that way, and therefore it is not necessary to discuss that feature of the question; we waste time. I am distinctly in favor of lessening the rigidity on No. 2 corn during the period of navigation, except during the period we call the germinating season. When you get beyond that season and stand here as I have day after day and seen corn inspected No. 3 that is as good No. 2 corn as was ever put on any market, we think it is time that Chicago should at least consider that she is not the whole thing in the United States. There are competitive markets around us that are drawing grain of all kinds away from us, wheat, corn, and I think barley, too; but these I come in personal contact with every day. We still cling to the idea that No. 2 must be almost spotless, must be hard as wheat, and must be almost absolutely free from dirt. Now, I under-

take to say Mr. Chairman and gentlemen, that when you passed this inspection rule you never contemplated the word dry to mean what it is being made to mean every day here. It did not mean flint dry, and it did not mean wood dry; it means commercially dry—a safe corn to handle wherever it is handled. I am going to give you a personal experience. In the last crop year I shipped, myself, over 10,000,000 bushels of corn. I sold it in Europe as No. 2 corn. What was the result? Every bushel graded No. 2, without any exception and we had no complaints from any of that corn. I wish to say that there was not one bushel of No. 2 corn in the lot. We are here to get all the corn we can to Chicago and give it fair inspection—inspection that is fair to the buyer and fair to the seller, and if the experience I have just given can't help us to arrive at solution of this question, I would like to know what will. When we have a corner here you can't get corn to grade No. 2. During the corner last summer I saw No. 3 corn that was as good corn as you ever saw. If this thing continues they will be in condition where they will ship here when they can, but this is not good for your department; it is not good for the seller, and is not good for the warehouseman. I say corn should be commercially dry and sound enough to ship to a warehouse. The fact is that when we have a corner here, as we seem to have this year every month, the inspectors are wrought up about it, but how about the man getting his little salary? He simply says No. 3 to avoid responsibility. Every man knows that is a fact. Why should we have a grade so much higher than is necessary? If a few grains are damaged is it right to condemn that entire carload of corn for what might represent half a peck in a 1,500-bushel car? I believe we should have strict inspection, but I do not believe we should have such an inspection as will deter people from sending corn here, and so that men running corners can take advantage of it. I do not believe in such inspection as invites and assists and makes it possible to run corners. I say that the inspection here except in the germinating season is too high and unjust.

Mr. French: As I understand the complaint it is with the inspection and not with the rules.

Mr. Counselman: Yes, sir. It is not necessary to change the rules.

Mr. French: Your complaint is with the way inspectors construe the rules and not with the rules?

Mr. Counselman: That is right, sir. I do not think that those who framed the rule meant it to mean so dry. I believe they meant it should be commercially dry. If you will say commercially dry you will let that man use his judgment a whole lot.

Samples of the different grades of grain were brought in and all given an opportunity to inspect them.

Mr. Bartlett: Mr. Counselman stated that the corn ought to be commercially dry. I do not know what that is. He says we do not want flint dry or wood dry, but commercially dry corn. The grade we have now is safe to buy and is safe to trade in, but the moment you advertise to the public you have changed from the present standard to commercial dry corn, while you may be right in that decision, it is going to take years to educate them, and in the meantime the Board of Trade will be paying the bills. There is only one thing for the inspection to do and that is to maintain the present stan-

dard of No. 2 corn. The moment you let the public suspect you are thinking of changing your degree of dryness your trade is gone.

Mr. Slaughter: I want to say that the complaints that have come to us have not been in regard to dryness, but that the inspection is too rigid in regard to dirt and damaged corn.

Mr. Eckhardt: I think it would be a mistake to change the rules in regard to dryness. The complaints have been chiefly in regard to damaged corn and dirt. I think the department is all right in regard to dryness.

Mr. Stibbens: I think there is a matter I ought to call attention to, as I hear complaints of the country shipper, the receiver and of the eastern buyer. It is commonly suggested that the in inspection and out inspection in Chicago are two different propositions. I think this the proper time and place to call attention to this. I have talked to eastern buyers and they say that the only way they will buy grain in Chicago is on sample. If there is anything wrong about the out inspection of corn in this market it should be remedied, as it is a very important matter with this market as well as with the eastern buyer. This market will lose trade if there is anything wrong with its inspection and I am told by eastern buyers that they will not buy grain in the Chicago market except by sample. It has been stated here that standard oats on the tables were selling at 4 cents higher than standard oats in store. Now why should that be so?

G. W. Patten: I am here as a representative of the Central Elevator Company. I think we have not sold any corn this year except by Chicago inspection. I think we all agree in regard to the dryness. In regard to dirt; it has come to be a condition here that everybody bids for 3 corn or better. It is no very big inducement for the farmer to clean his corn. The result is that they get careless and sometimes I think they throw in a little dirt to make it 56 pounds to the bushel. It comes in here and if it grades 3 on account of dirt, it can be cleaned at small expense and made contract corn. I think the department did nobly during the corner. I think our inspection is the best in the world.

Mr. French: What has been your observation in regard to grade of grain coming in and that going out?

Mr. Patten: I think they do not know what they are talking about when they say corn grades in and won't grade out.

Geo. E. Marcy: I think it would be almost suicidal to the market to interfere with the present grade of corn. I do think, however, that the department has at times become a little too rigid on grading of corn. But I think this is the case only on some of the roads and it is on account of there being different inspectors. I think that is caused by the different inspectors not lining up with the supervising inspectors as well as they ought to. I will say that the statement that the out inspection is not equal to the in inspection is not a fact. Certainly as far as inspection goes it is far ahead of anything in the country. You cannot rely on any other inspection, except Minneapolis and Duluth. The inspection of Philadelphia and Baltimore are made to fit conditions. They make one grade for seed and another for grain to go in store.

Mr. French: Is there any difference in the in inspection and the out inspection?

Mr. Marcy: No, sir, I do not think so.

I think where people get that impression is by corn coming in here a little dirty, being cleaned up, and when it goes out it is cleaned and made better and grades better than when it came in.

F. H. Hancock of the Des Moines Elevator Co., was present and was asked to give his opinion of Chicago inspection. He said: I do not know that I have anything to say about this matter. I am a country dealer in the state of Iowa. So far as the inspection is concerned I am perfectly satisfied with it.

Mr. French: How many places do you handle grain at?

Mr. Hancock: At two places principally, at Des Moines and Moberly, Mo. We have about thirty stations throughout the country. I have been in the business in Iowa for the last thirty years. I am very well satisfied with the Chicago inspection. I can only add to what has already been said. The farmers are very careless with their corn, and that is one of the causes of corn grading No. 3 as a rule. The difference in price is not sufficient to prompt them to clean their corn, there being only $\frac{1}{4}$ or $\frac{1}{2}$ cent difference.

Mr. Warren: I want to see the time come when the inspection certificate of the State of Illinois will be all that is called for in the Chicago market.

Mr. Patten: Why not take a vote on how many present want the grade lowered?

Mr. Slaughter: I would suggest that Mr. Warren, President of the Board of Trade, appoint a committee composed of a number of those present to meet with the inspectors and determine on standard grade on all grains.

Mr. Patten: I move that it be the sense of this meeting that the present standard of No. 2 corn should be maintained.

The motion was carried unanimously.

Mr. French stated he was glad they had all come together and expressed themselves, and that they all agreed on the point under discussion.

M. Witte, Russian minister of finance, has directed that every vessel carrying Russian grain shall be furnished with an official certificate of the weight and quality of its cargo. Cargoes of wheat have been found to contain 18 per cent of dirt.

The yield of barley as estimated by the Department of Agriculture Oct. 10 is 29 bushels, against 23.3 the average of 10 years. The yield of rye is 17 bushels, compared with a 10-year average of 14.4 bushels. The condition of buckwheat on Oct. 1 was 80.5, compared with 86.4 a month earlier and 80.2 the average for 10 years. As to the condition of rice, Texas reports 11 points above and South Carolina 1, Florida 5, North Carolina 8, Alabama 12, Mississippi 17 and Louisiana 23 points below their respective averages for the last eight years, while the condition in Georgia is average.

Bean prices since 1872 have fluctuated within limits of \$3.65 (in April and July, 1882) and \$.85 (in April and July, 1887). In the early 70's the market price at New York gradually declined from \$3 in 1873 and 1874 to \$1 in July, 1876, per bushel. A year later the price reached \$2.85, declined to \$1.30 in July, 1880, rose to \$3.65 two years later and in April of 1885 and 1886 again fell to the low price of 1880. Since the rise to \$2.70 in April, 1888, beans have never again risen to that point, the nearest approach to it after the record low point of \$.85 being \$2.25 in Oct., 1901.

GRAIN CARRIERS.

Lake grain rates at Chicago have advanced to 1 $\frac{1}{8}$ cents to Buffalo.

The Pennsylvania Railroad has extended the ex-lake grain rates from Erie, Pa., to Baltimore, Md., until Nov. 15.

The Joliet & Western, J. R. Steward of Plano, Ill., president, proposes to build 70 miles of road from Joliet to De Kalb.

Work is progressing on the 133 miles of road to be built for the El Paso & Rock Island from Dawson, N. M., to Tucumari.

In the sinking of the barge Missouri at Sacramento, Cal., recently, with 14,000 bags of wheat, 3,000 of the sacks were lost.

A score of steamers are offering to load grain at the Atlantic ports at the prevailing low rates without being able to get a cargo.

The arrival last week at Chicago of two cargoes of flaxseed from Duluth and Superior is the evidence of an unusual condition.

Kansas City grain dealers complain that cars cannot be obtained for the shipment of large quantities of wheat they have sold to outside mills.

Delay in delivery of grain by rail carriers in all parts of the country has reached the chronic stage. Grain shippers everywhere are calling for relief.

Impending strikes by western railroad workers should caution grain shippers against binding themselves to deliver grain at a distant market too far ahead.

The 7,000-ton steamer Minnetonka, which was built in sections at Cleveland, O., has been put together after passing thru the Canadian canals and has just finished loading at Montreal.

Average weekly shipments of grain by lake this season prior to Sept. 1, as reported by O. P. Austin, chief of the bureau of statistics, have been 1,423,000 bushels, compared with 2,235,000 bushels last year.

When a shipper makes requisition for a car of 50,000 pounds capacity and is furnished one of almost double the needed size, what right has the railroad company to compel him to load the car to its capacity?

Attorney General Knox in his recent speech at Pittsburgh argued that the law and Congress are competent to wipe out railroad discrimination and monopoly. Nothing is wanted more earnestly by the grain dealers of the country.

Average shortages in American grain cargoes unloaded at the elevators at Manchester, Eng., are .43 of 1 per cent for all grain, as reported by the Manchester Ship Canal Co. The shortage on wheat averages .36, on corn .78 and on oats .68 of 1 per cent.

Lack of motive power, which showed itself last season, is more apparent than ever to the close observer this season, since the per diem rule of payment for borrowed cars has improved the supply of the latter. Some yards are almost blocked with cars waiting for locomotives to move them.

Major Thomas W. Symons, government engineer, has ably defended the proposed route for the enlarged Erie Canal through the interior of the state by way of the Seneca and Oneida rivers, Oneida Lake and the Mohawk River, and not by the Lake Ontario route, and in a published statement gives reasons therefor.

The British "freak" steamers, as they are called, the turrets, that came across this season to do up our lake craft will

probably give this port a wide berth hereafter, with no love lost on either side. The Turret Cape brought into Buffalo a cargo of 105,000 bushels of Canadian wheat and has just managed to get rid of it after several days of failing to get to an elevator, on account of deep draft. Our own steamers are good for three times that cargo and do not go on the bottom either.—J. C.

The plan of running canal boats from Lake Erie ports to Buffalo and thence thru the Erie Canal has been thoroughly tested and found impracticable. The fleet of steel boats that was built for this service has been sold and taken out of the country. The perils of navigating Lake Erie were the cause of the failure. The plan of moving the boats over the lake protected by a large floating dock was considered and plans were drawn for such a craft, but it was never built.

About 50 receivers, elevator men and millers attended the meeting of the Missouri Railroad and Warehouse Commissioners at St. Louis, Mo., Oct. 10, to protest against the reconsignment charge of \$2 per car at St. Louis, Kansas City and St. Joseph. The commissioners laid special stress upon statements made to them by representatives of the railroads as to the practice in other markets, and disclaimed any intention of placing difficulties in the way of doing business. Frank Goodenough, representing the Mississippi Valley Elevator, said the order would practically put his company out of business, as under it grain could be consigned to the east side and sent to the elevators without this special charge of \$2 per car. Governor Stanard urged the commission to withdraw the order, which he considered a special burden on the St. Louis market.

By fraudulent mixture of dirt the smaller exporters of barley in Russia are said to have gained possession of the trade to the exclusion of the large firms.

Corn oil to the amount of 2,258,617 gallons was exported during the eight months ending Aug. 31; compared with 3,135,744 gallons exported during the corresponding eight months of 1901.

The bill establishing a department of commerce will be presented in completed form when Congress meets in December. The bill is expected to pass early in the short session. As prepared by the committee on interstate and foreign commerce the bill has the support of interested cabinet officers and President Roosevelt.

Frank H. Mason, American consul at Berlin, in a recent letter to the state department, takes a pessimistic view of the cereal harvests in Germany. As the time came for the wheat and rye to ripen, the cold, wet, cloudy weather kept back the harvest long after the usual date, and, when the work of gathering in the grain finally began, greatly complicated the task of saving the crops. In many fields the rye and wheat have been cut and lain in swathes or put up in shocks, exposed to rain and storm, until the grain has been seriously damaged by sprouting. The first samples of the new-crop rye which have been received at the Produce Exchange in Berlin are almost without exception damp, more or less unripe, and, even when artificially dried, of inferior grade. While, therefore, the German grain crop this year will be up to the full average in quantity, its quality has been more or less seriously damaged, and large quantities of rye, which should have been used as breadstuff, will now be distilled or used for feeding animals.

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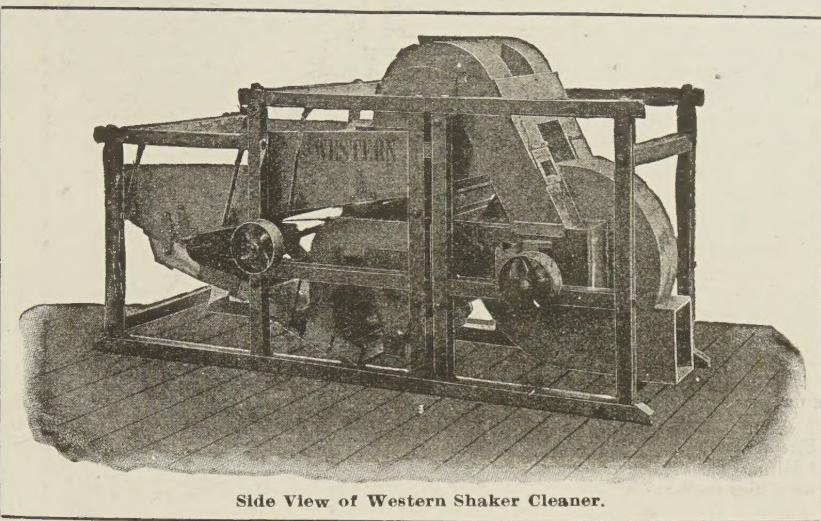
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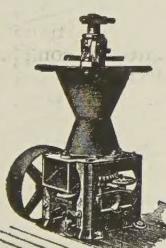
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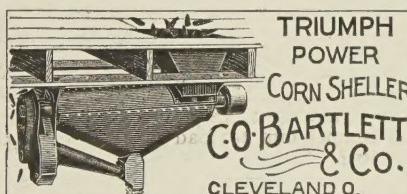
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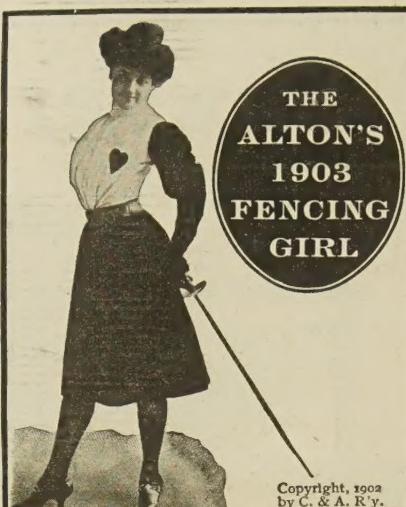
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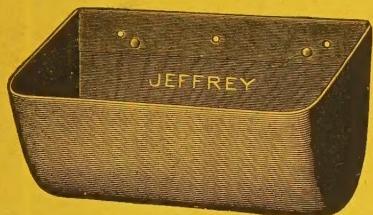
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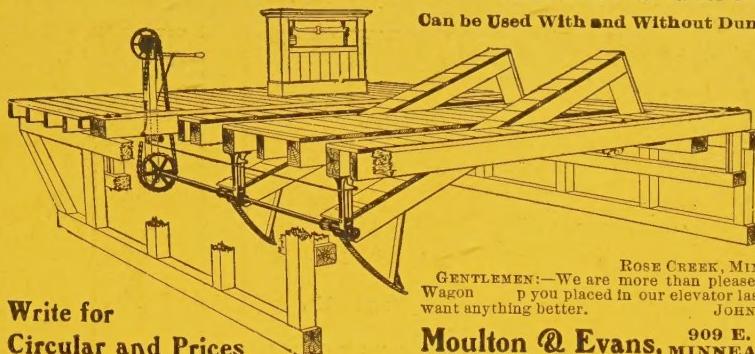
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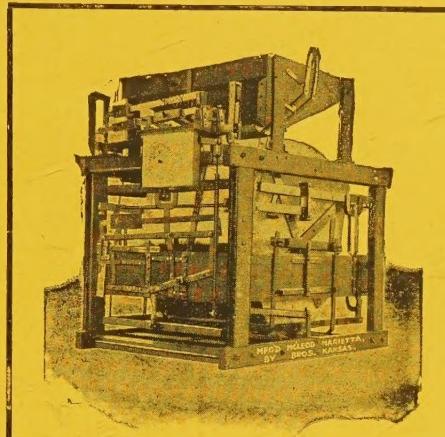
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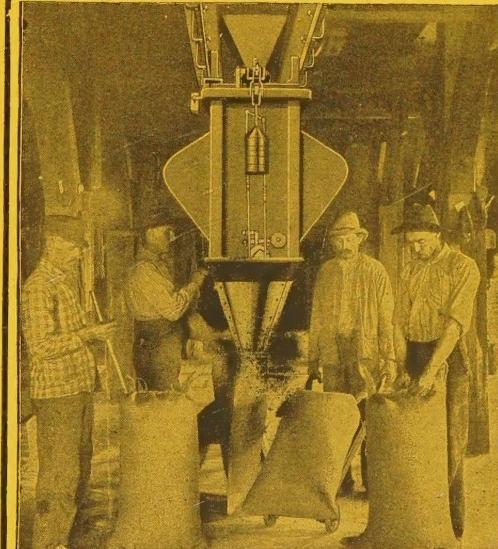
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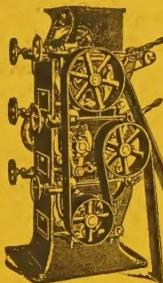
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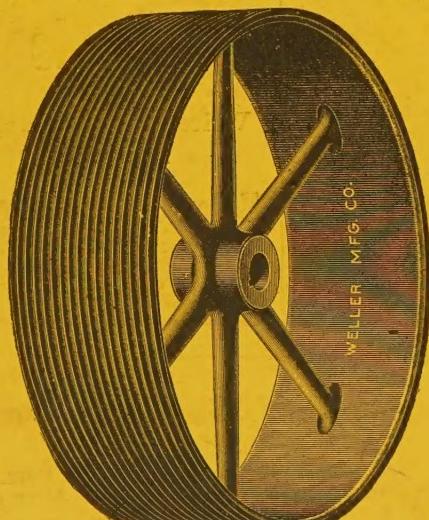


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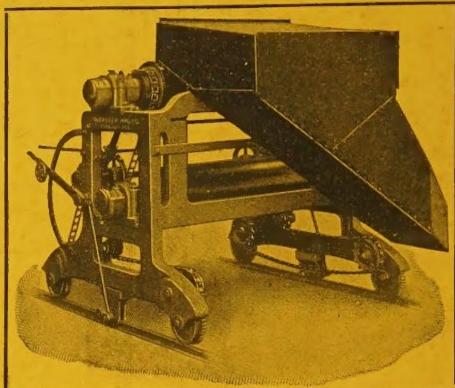
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